

**PUBLIC WORKS AND GOVERNMENT  
SERVICES CANADA**

**Royal Canadian Mounted Police  
RCMP Hangar  
190 Veteran's Way  
Aircraft Fueling System Modifications**

**Prince Albert, Saskatchewan  
Project No. R042523.006**

**ISSUED FOR TENDER**

DRAWING LIST

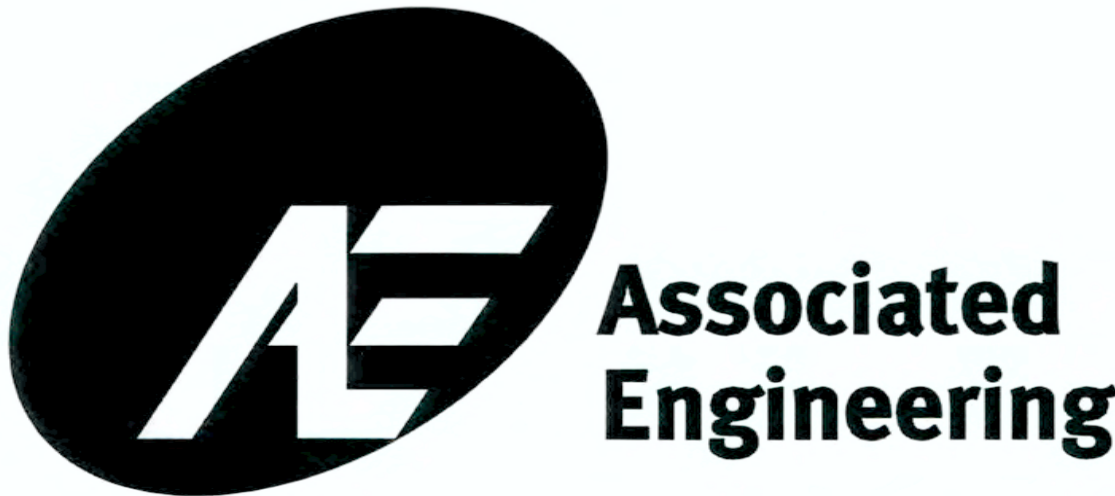
|                   |                      |
|-------------------|----------------------|
| <u>GENERAL</u>    |                      |
| TITLE             | COVER & DRAWING LIST |
| G-101             | LOCATION PLAN        |
| G-102             | GENERAL ARRANGEMENT  |
| <u>CIVIL</u>      |                      |
| C-101             | EXCAVATION PLAN      |
| C-102             | PLAN & DETAILS       |
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| S-101             | PLAN & DETAILS       |
| <u>MECHANICAL</u> |                      |
| P-101             | PLAN & DETAILS       |
| <u>ELECTRICAL</u> |                      |
| E-101             | DETAILS              |



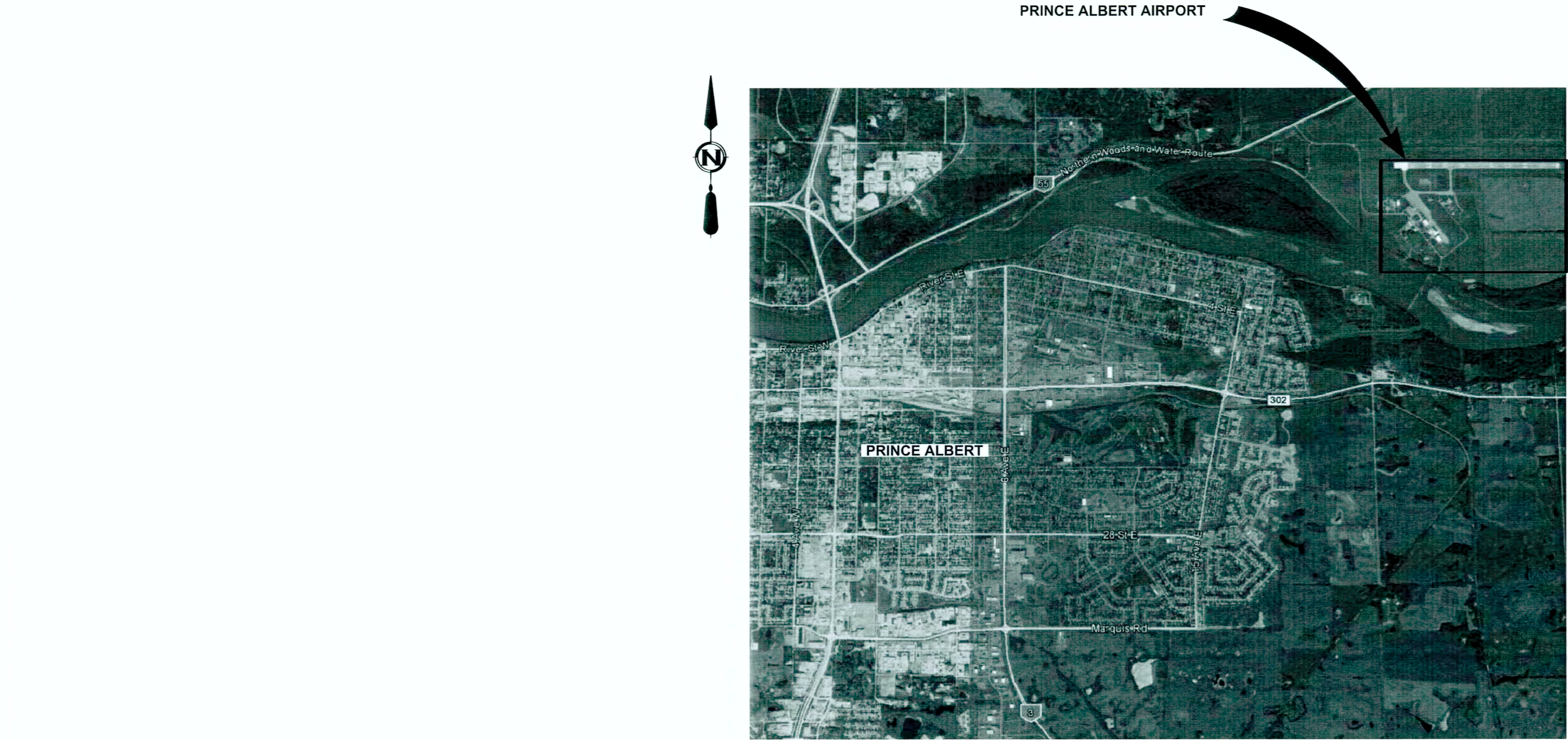
Public Works and  
Government Services  
Canada

Travaux publics et  
Services gouvernementaux  
Canada

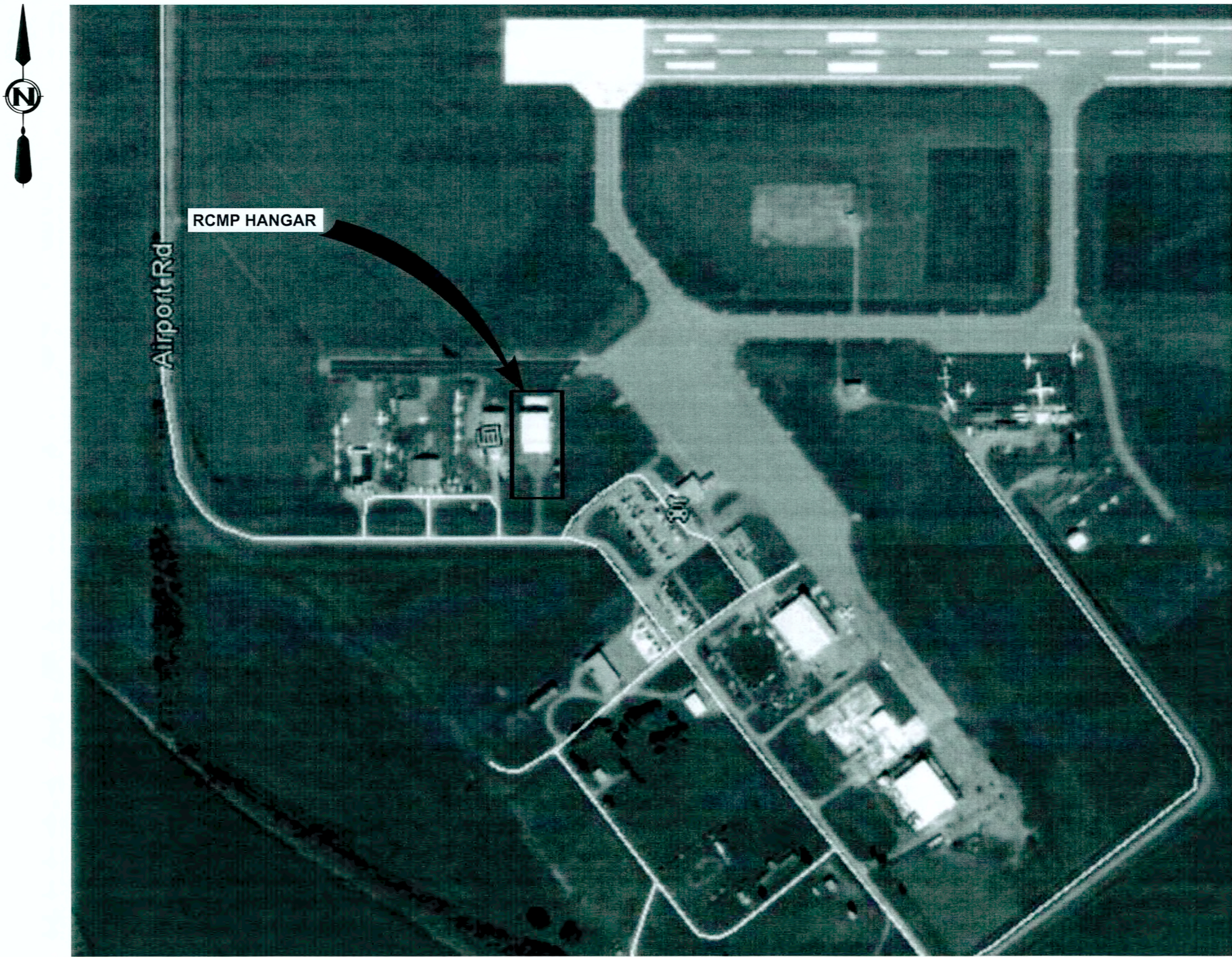
**EGE Engineering Ltd.  
Engineering,  
Geosciences and  
Environmental**



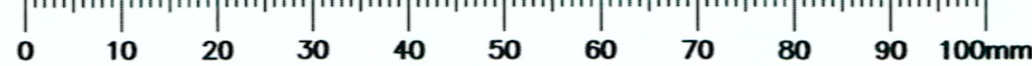
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| ASSOCIATED<br>ENGINEERING<br>QUALITY<br>MANAGEMENT | QUALITY MANAGER<br><i>[Signature]</i> | DATE<br>2016-06-23 | PROJECT CAD LEAD<br><i>[Signature]</i> | DATE<br>2016-03-22 |
|--|---------------------------------------|--------------------|--|--------------------|



1 PLAN NTS  
PRINCE ALBERT AIRPORT  
LOCATION PLAN



2 PLAN NTS  
PRINCE ALBERT RCMP  
HANGAR LOCATION PLAN



| Revision | Description       | Date    |
|----------|-------------------|---------|
| 0        | ISSUED FOR TENDER | 6/15/16 |

Client client

Project title Projet

**AIRCRAFT FUELING SYSTEM  
MODIFICATIONS  
RCMP HANGAR  
PRINCE ALBERT, SK**

Designed by Conçu par  
M. AKISTER

Drawn by Dessiné par  
N. MARKHAM

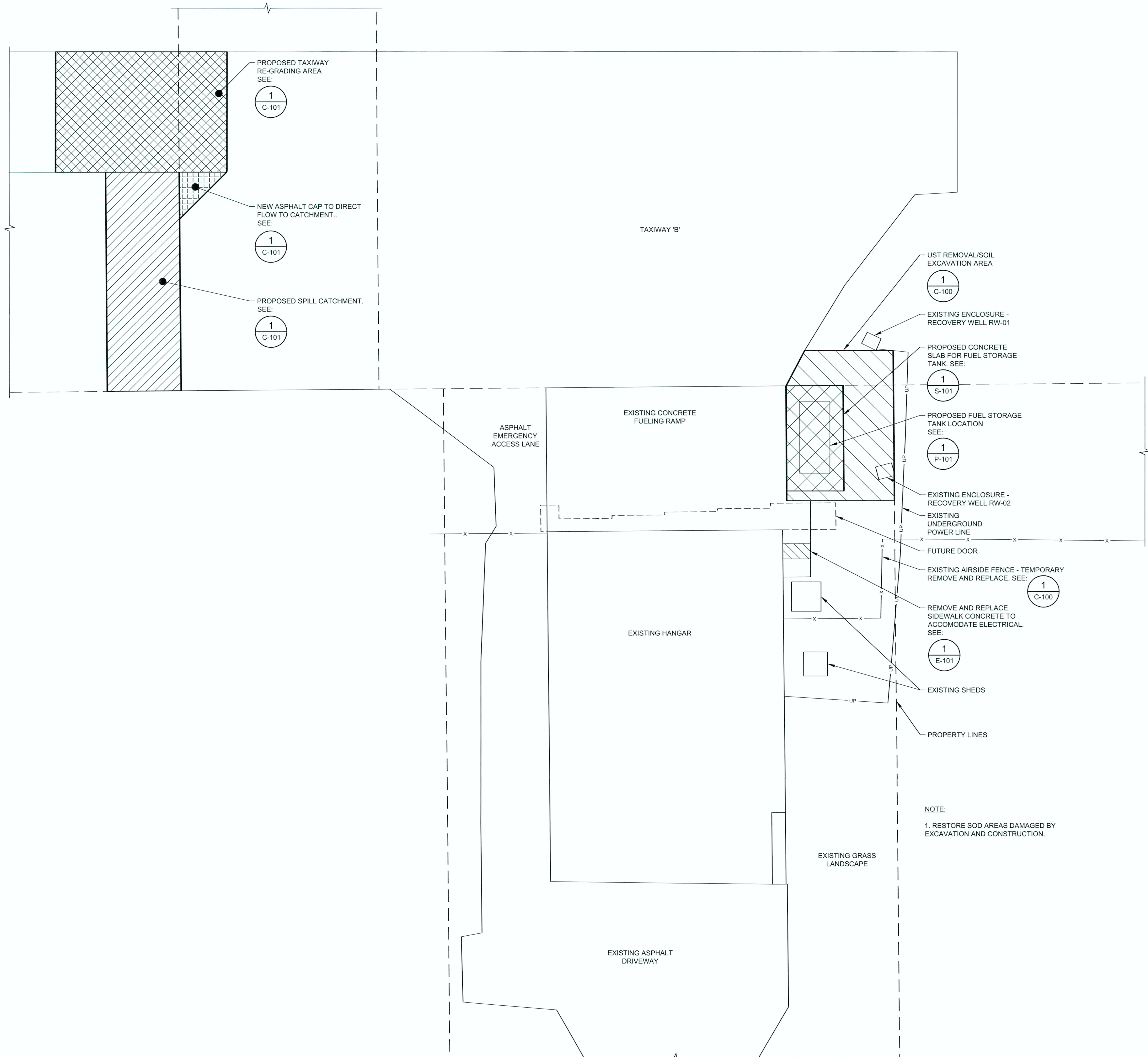
Approved by Approuvé par  
J. MEDORI

PWSSC Project Manager Administrateur de Projets TPSGC  
J. LA RUE-VAN ES

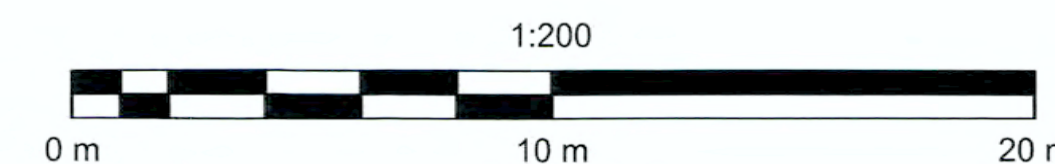
Drawing title Titre du dessin

**LOCATION PLAN**

| Project no./No. du projet | Drawing no./No. du dessin | Revision no. |
|---------------------------|---------------------------|--------------|
| R042523.006               | G-101                     | 0            |



1 PLAN  
- SITE PLAN 1:200



Public Works and  
Government Services  
Canada  
Travaux publics et  
Services gouvernementaux  
Canada  
REAL PROPERTY SERVICES  
Western Region  
SERVICES IMMOBILIERS  
Région de l'ouest

**Associated Engineering**  
1 - 2225 Northridge Drive, Saskatoon, Saskatchewan, S7L 6X6  
Ph: 306 653 4969 Fax: 306 242 4904  
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NUMBER C116  
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DISCIPLINE: Civil SASK. REG. NO. 20401 SIGNATURE: J.C. Medori

| Revision | Description       | Date    |
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| 0        | ISSUED FOR TENDER | 6/15/16 |

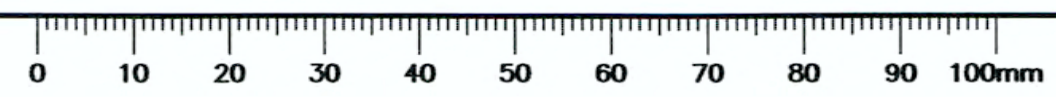
Client client

Project title Projet  
**AIRCRAFT FUELING SYSTEM  
MODIFICATIONS  
RCMP HANGAR  
PRINCE ALBERT, SK**

Designed by Conçu par  
M. AKISTER  
Drawn by Dessiné par  
N. MARKHAM  
Approved by Approuvé par  
J. MEDORI  
PMGSC Project Manager Administrateur de Projets TPSGC  
J. LA RUE-VAN ES  
Drawing title Titre du dessin

**GENERAL  
ARRANGEMENT**

| Project no./No. du projet | Drawing no./No. du dessin | Revision no. |
|---------------------------|---------------------------|--------------|
| R042523.006               | G-102                     | 0            |





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Project title Projet

**AIRCRAFT FUELING SYSTEM  
MODIFICATIONS  
RCMP HANGAR  
PRINCE ALBERT, SK**

Designed by A. PASSALIS (EGE) Conçu par

Drawn by N. MARKHAM Dessiné par

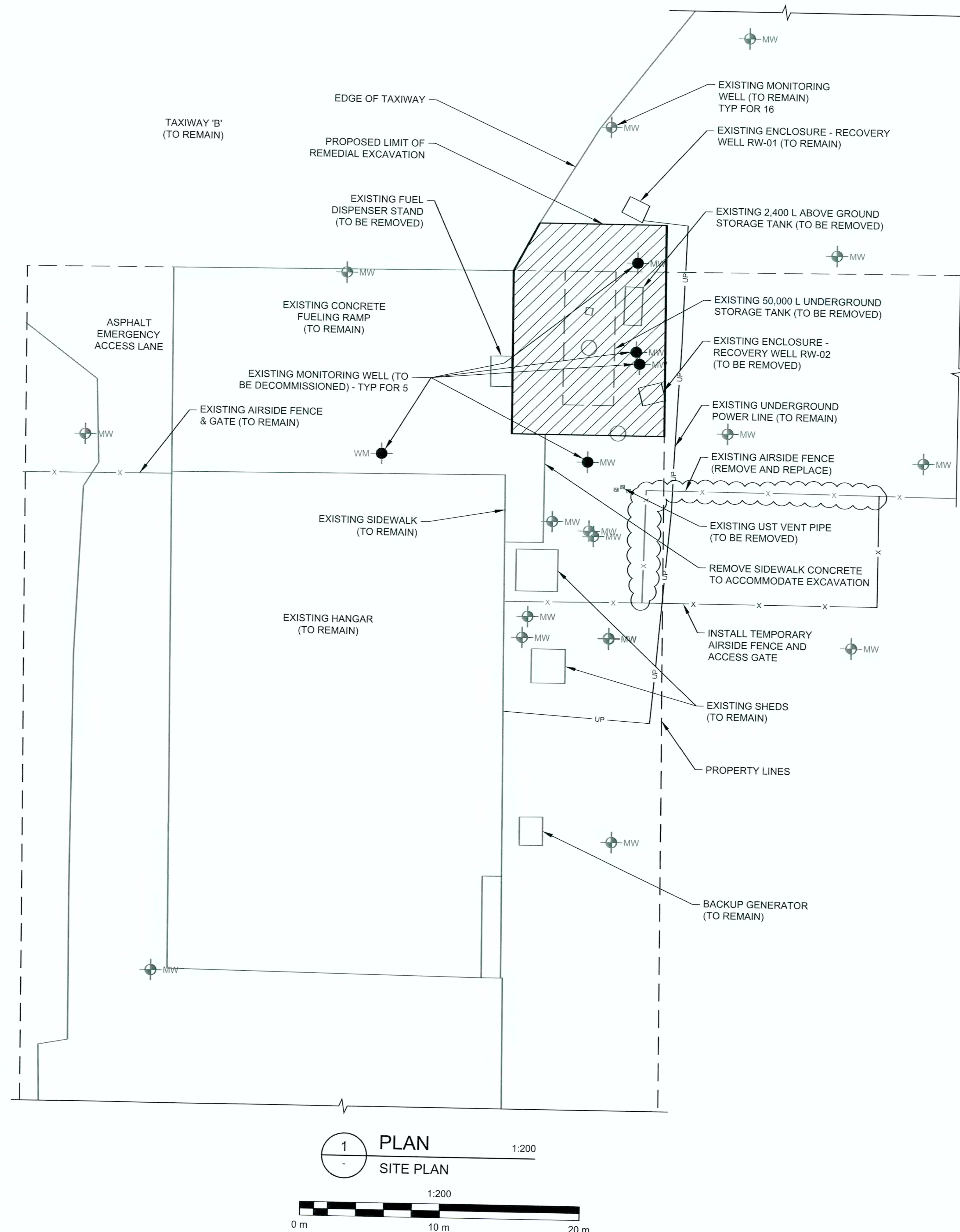
Approved by J. MEDORI Approuvé par

PMSCS Project Manager J. LA RUE-VAN ES Administrateur de Projets TPSGC

Drawing title Titre du dessin

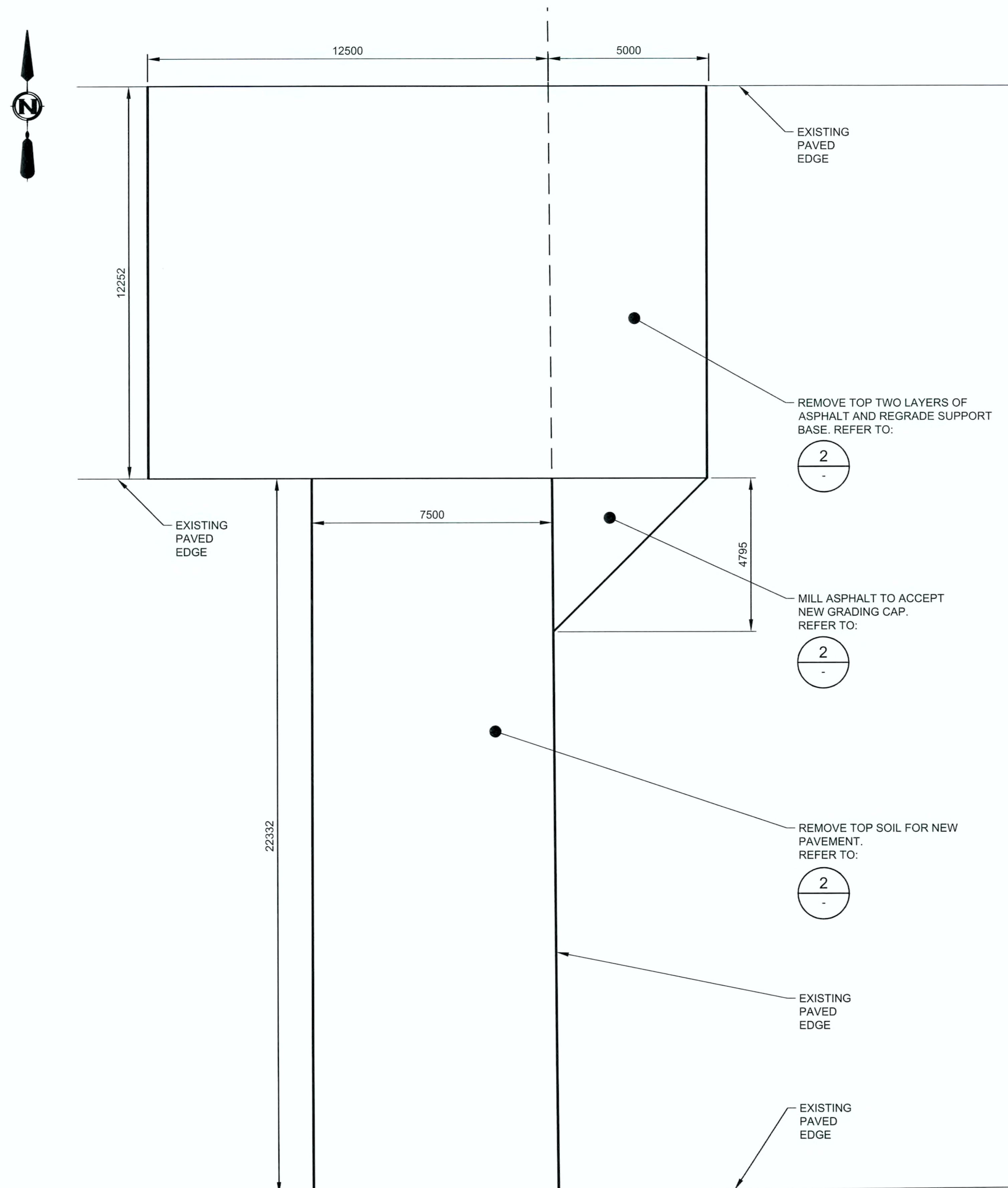
**EXCAVATION  
PLAN**

|                           |                           |              |
|---------------------------|---------------------------|--------------|
| Project no./No. du projet | Drawing no./No. du dessin | Revision no. |
| R042523.006               | C-101                     | 0            |

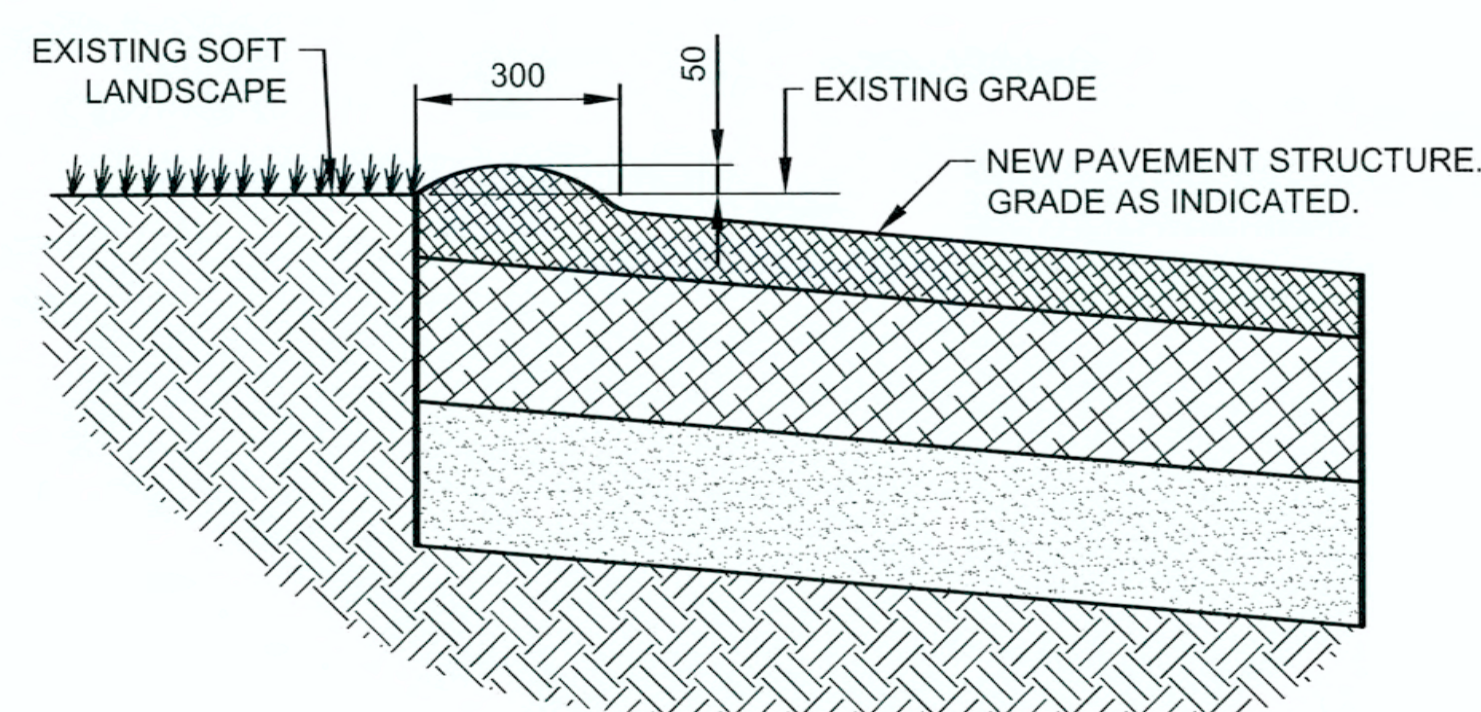
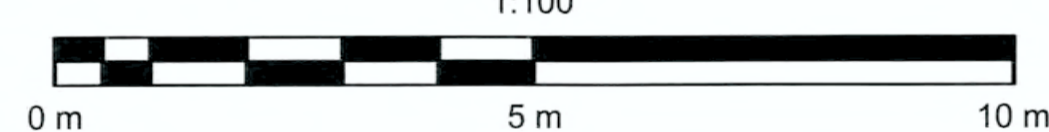


**NOTES:**

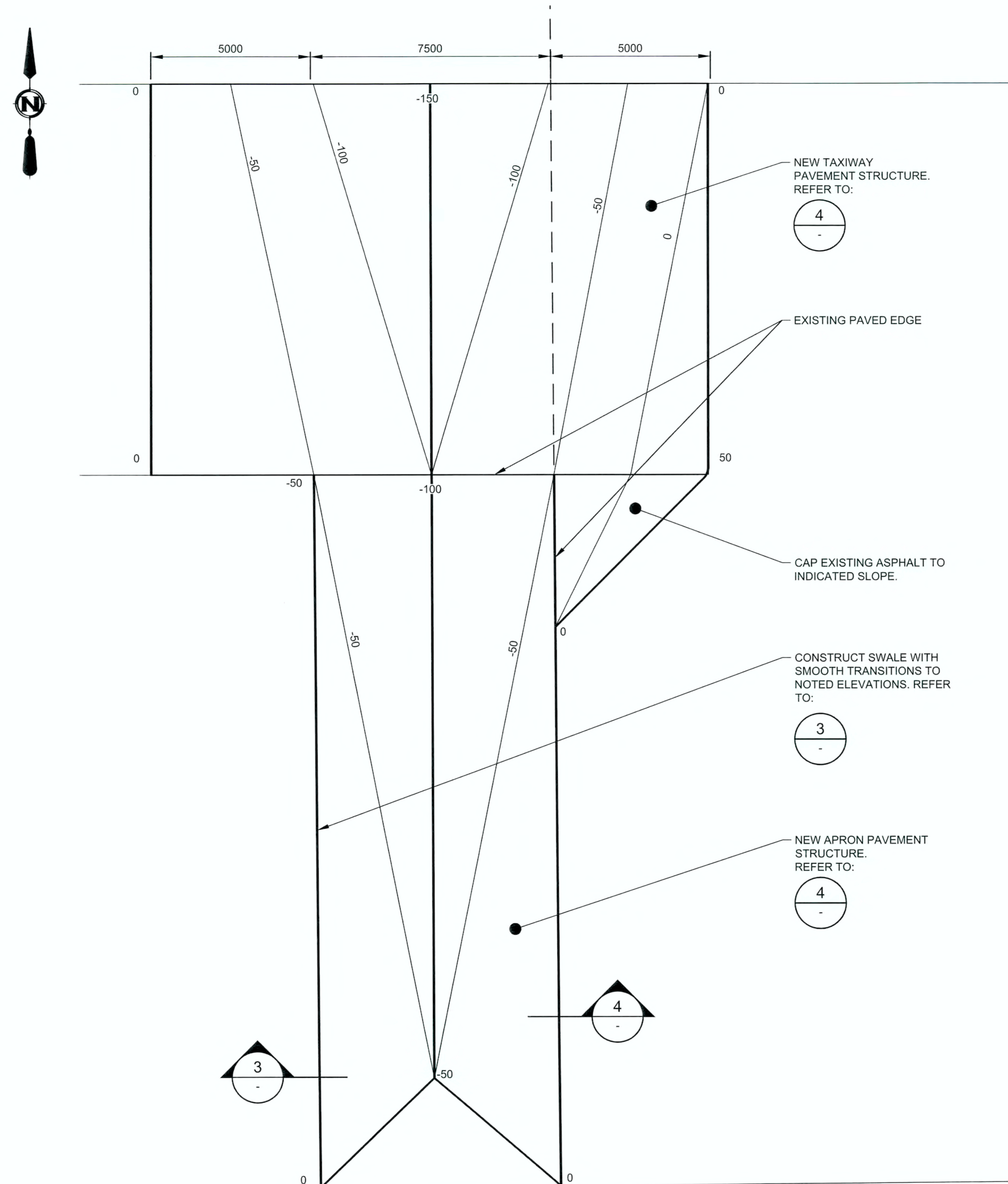
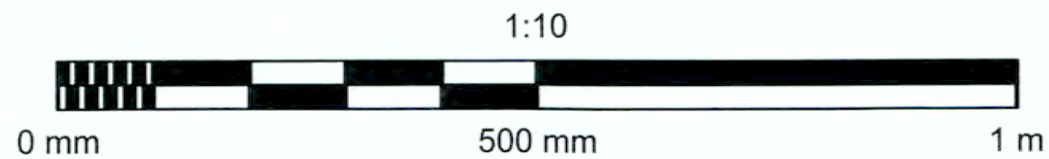
- SHORING AND PROTECTION OF EXCAVATIONS ADJACENT PROPERTIES, AND EXISTING STRUCTURES TO PRE-CONSTRUCTION CONDITION REMAIN THE RESPONSIBILITY OF THE CONTRACTOR.
- DESIGN AND PROVISION OF SHORING REMAINS THE RESPONSIBILITY OF THE CONTRACTOR.
- THE SHORING DESIGN ENGINEER IS RESPONSIBLE TO ENSURE THAT ALL EXISTING STRUCTURES AND PAVED SURFACES ARE TO REMAIN WITHOUT DISRUPTION OF SERVICE. CONTRACTOR TO MAINTAIN THE INTEGRITY OF ALL BUILDINGS AND PAVED SURFACES ONSITE TO PRE-CONSTRUCTION CONDITIONS THROUGH THE USE OF SHORING AND PROTECTION DURING EXCAVATION AND CONSTRUCTION. ENGINEER SHALL SUBMIT SHORING DETAIL DESIGN DRAWING(S) TO THE DEPARTMENTAL REPRESENTATIVE FOR REVIEW PRIOR TO PROCEEDING WITH THE WORK. THE DESIGN AND DRAWINGS SHALL BE SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE PROVINCE OF SASKATCHEWAN AND HOLDS A CURRENT "CERTIFICATION OF AUTHORIZATION" OF APEGS.
- PROTECT ALL MONITORING WELLS NOT IDENTIFIED TO BE DECOMMISSIONED.



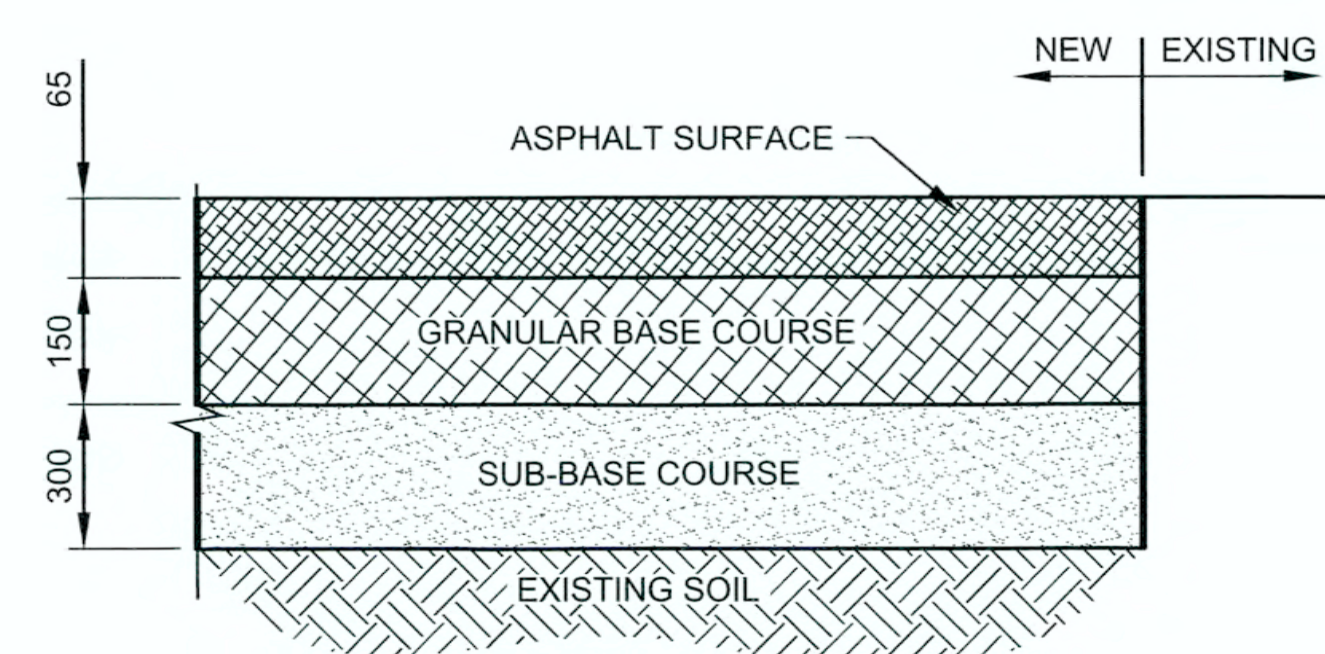
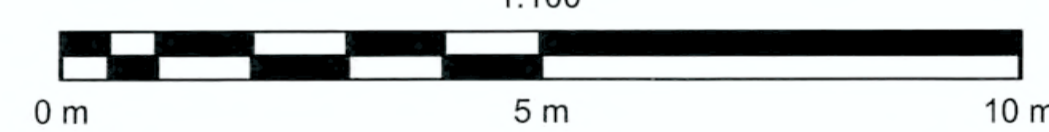
1 DETAIL 1:100  
- SITE DEMOLITION PLAN



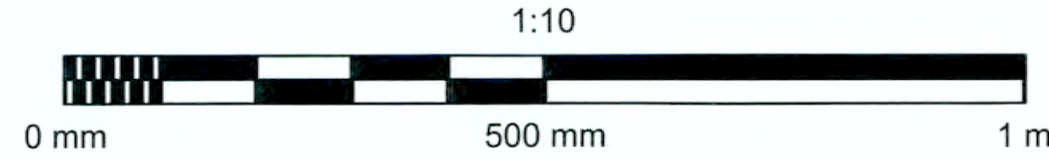
SECTION 1:10  
ASPHALT SWALE



2 DETAIL 1:100  
- SITE PLAN

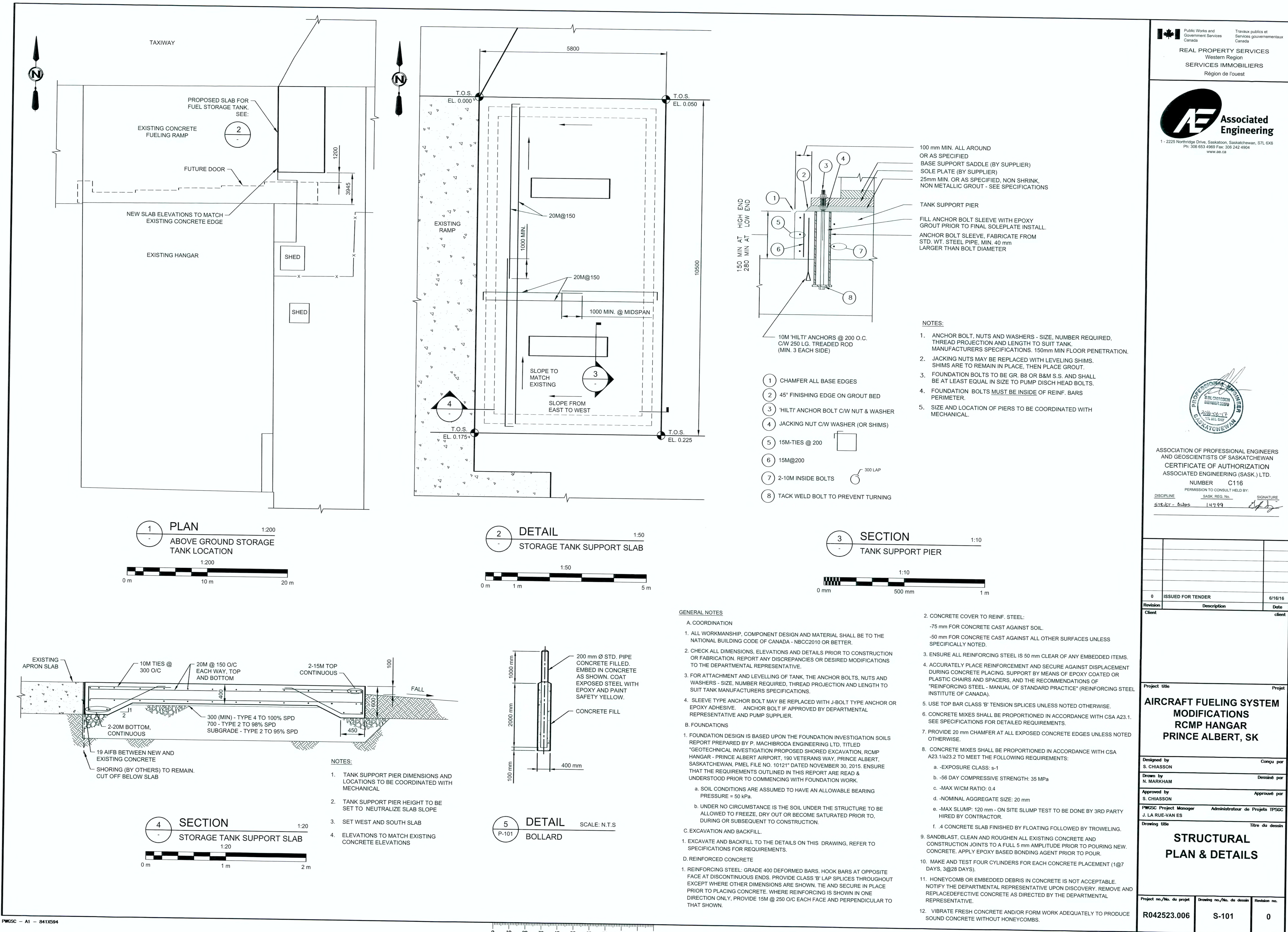


4 SECTION 1:10  
PAVEMENT STRUCTURE



- NOTES:

1. EXISTING ASPHALT BASE MATERIAL CAN BE SALVAGED FOR USE AS NEW SUPPORT BASE MATERIAL AS REVIEWED AND ACCEPTED IN WRITING BY THE DEPARTMENTAL REPRESENTATIVE
2. ASPHALT CAP AREA TO BE ESTABLISHED IN FIELD BASED ON EXISTING ELEVATIONS
3. SUBGRADE TO BE INSPECTED BY ENGINEER AFTER TOPSOIL STRIPPING AND PRIOR TO PLACEMENT OF GRAVEL MATERIAL. IF IRREGULARITIES SUCH AS ORGANICS OR SOFT SPOTS, THE EXISTING MATERIAL WILL BE REMOVED AND REPLACED WITH COMPETENT MATERIAL AS DIRECTED BY THE ENGINEER.
4. SAW CUT EXISTING ASPHALT PAVEMENT AS DIRECTED BY ENGINEER TO PROVIDE SMOOTH SURFACE FOR BUTT JOINT.
5. APPLY TACK COAT TO EDGE OF EXISTING ASPHALT PRIOR TO PLACING NEW ASPHALT
6. THE TRANSITION FROM ONE SLOPE TO ANOTHER SLOPE SHOULD BE ACCOMPLISHED BY A CURVED SURFACE WITH A RATE OF CHANGE NOT EXCEEDING 5 mm IN A 4.5 m STRAIGHT EDGE
7. REPAINT EXISTING 150 mm WIDE YELLOW MARKING ALONG EXISTING TAXIWAY CENTRE LINE



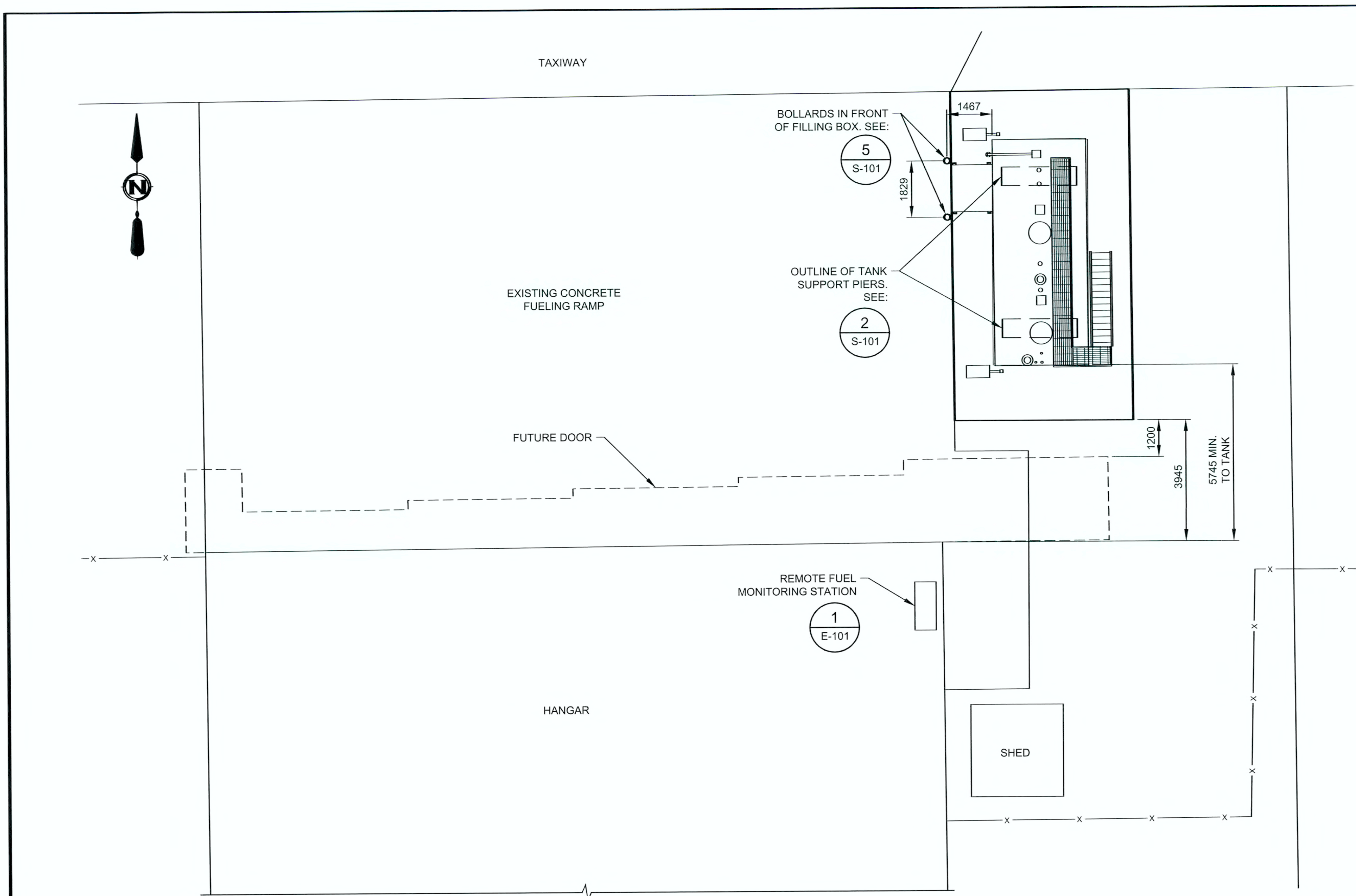
| Revision | Description       | Date    |
|----------|-------------------|---------|
| 0        | ISSUED FOR TENDER | 6/16/16 |

**AIRCRAFT FUELING SYSTEM MODIFICATIONS  
RCMP HANGAR  
PRINCE ALBERT, SK**

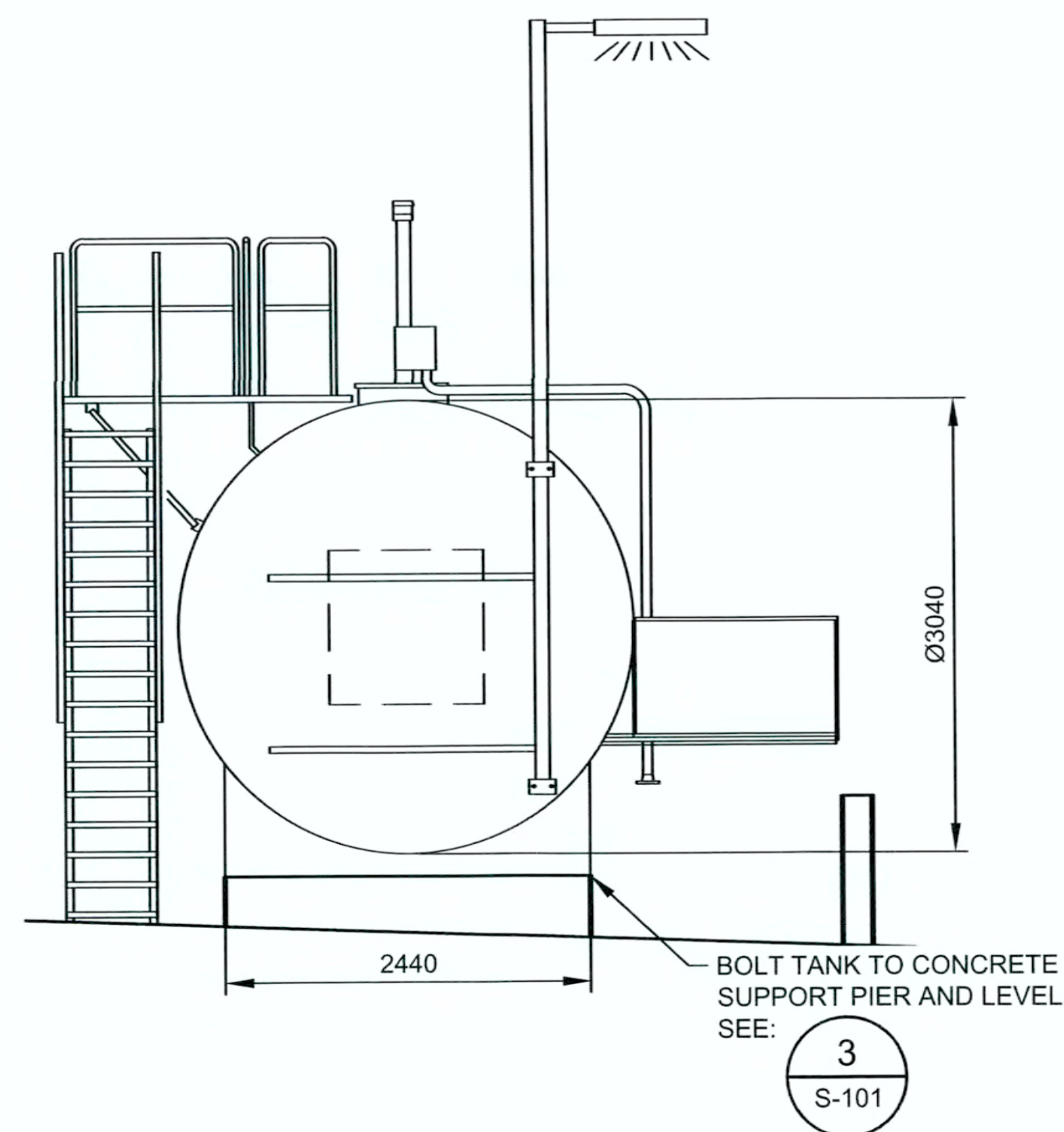
|   |                                 |
|---|---------------------------------|
| Designed by<br>S. CHIASSON                | Conçu par                       |
| Drawn by<br>N. MARKHAM                    | Dessiné par                     |
| Approved by<br>S. CHIASSON                | Approuvé par                    |
| PWSSC Project Manager<br>J. LA RUE-VAN ES | Administrateur de Projets TPSGC |
| Drawing title                             | Titre du dessin                 |

**STRUCTURAL  
PLAN & DETAILS**

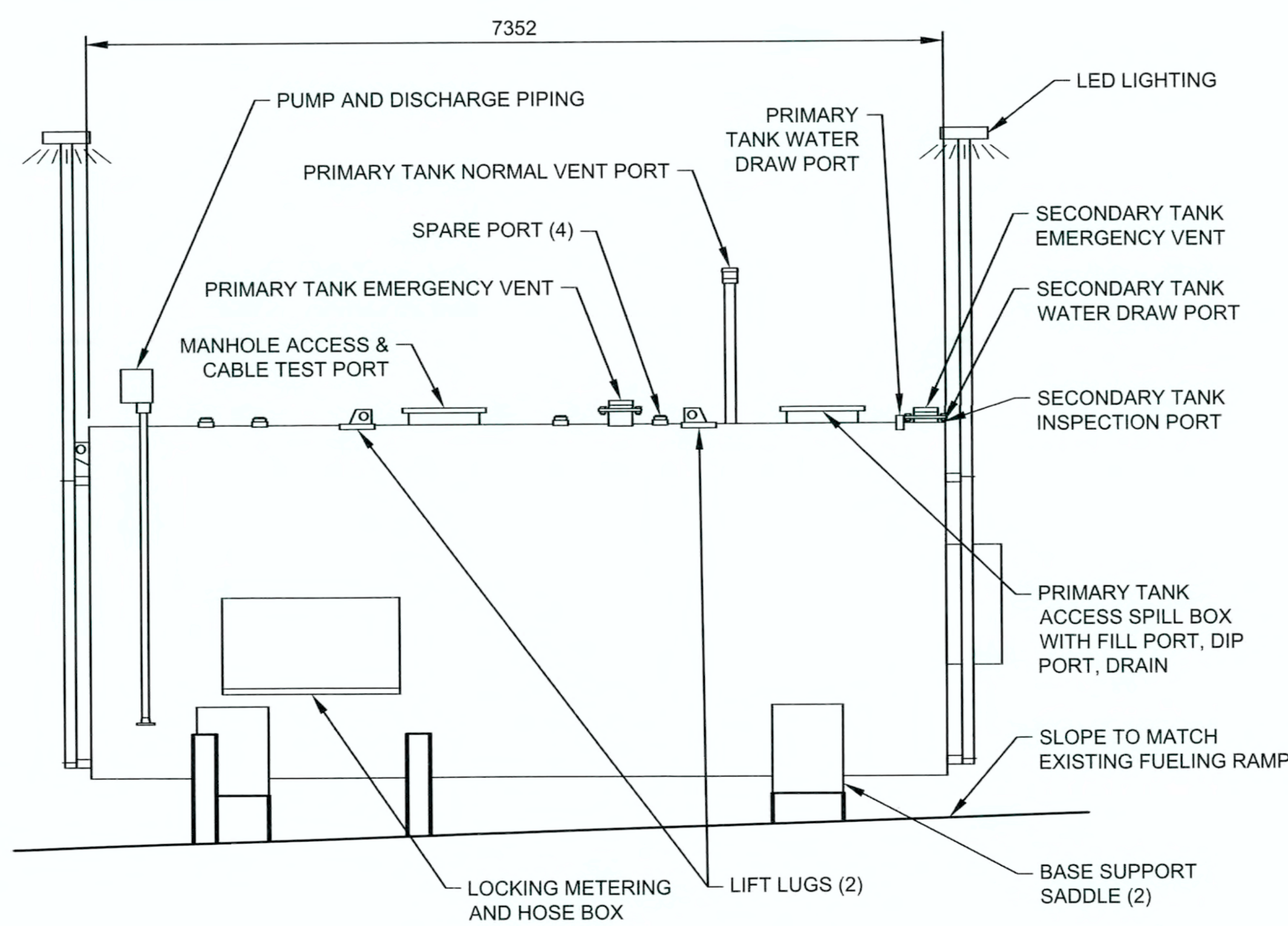
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| Project no./No. du projet<br>R042523.006 | Drawing no./No. du dessin<br>S-101 | Revision no.<br>0 |
|--|------------------------------------|-------------------|



1 PLAN  
ABOVE GROUND STORAGE TANK  
1:100  
0 m 5 m 10 m



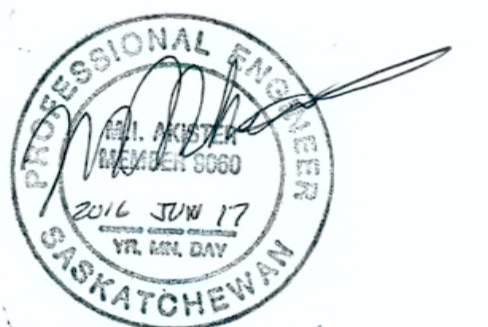
2 ELEVATION  
ABOVE GROUND STORAGE TANK SOUTH ELEVATION  
1:50  
0 m 1 m 5 m



3 ELEVATION  
ABOVE GROUND STORAGE TANK WEST ELEVATION  
1:50  
0 m 1 m 5 m

#### NOTES:

- ABOVE GROUND STORAGE TANK TO INCLUDE THE FOLLOWING:
  - 45,000 L CAPACITY FOR JET "A" FUEL
  - COMPLY WITH ULC-S601
  - 100% SECONDARY CONTAINMENT
  - 30m HOSE WITH MOTORIZED REEL AND DISPENSING METER IN LOCKING CABINET
  - ACCEPTABLE HOSES: API BULL 1529 TYPE C, F, OR CT
  - DUAL 2 STAGE FILTRATION WITH AUTOMATIC SWITCHOVER
  - ACCESS STAIRS AND INSPECTION PLATFORM TO PROVIDE ACCESS TO ALL TOP OPENINGS
  - CONTINUOUS PRODUCT LEVEL AND LEAK DETECTION MONITORING SYSTEMS, INCLUDING AUDIBLE AND VISUAL ALARMS, DISPENSING VOLUME RECORDING LOGS & REMOTE CONTROL CONNECTION PROVISIONS.
  - ON TANK LIGHTING
  - GRAVITY FILLING WITH SPILL BOX FOR TANK TRUCKS
  - SECURITY AGAINST THEFT AND VANDALISM
  - FACTORY PRE-WIRED
  - WEATHER PROOF CABINET FOR ELECTRICAL CONNECTIONS AND DISCONNECTS
- TURN OVER STATUS MONITORING AND DISPENSING RECORDER TO ELECTRICAL TRADE FOR INSTALLATION INSIDE HANGAR
- COORDINATE CONCRETE PIER DIMENSIONS AND ANCHOR BOLT PLACEMENT WITH STRUCTURAL
- BOLT TANK BASE SUPPORT TO CONCRETE PIER. LEVEL AND GROUT.
- COORDINATE SIZE AND LOCATION OF EACH PIER WITH STRUCTURAL TO MINIMIZE GROUTING REQUIREMENTS
- PAD ELEVATION TO MATCH FUELING RAMP



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Project title  
**AIRCRAFT FUELING SYSTEM MODIFICATIONS  
RCMP HANGAR  
PRINCE ALBERT, SK**

Designed by  
M. AKISTER  
Conçu par  
N. MARKHAM  
Dessiné par  
Approved by  
M. AKISTER  
Approuvé par  
PMWSC Project Manager  
J. LA RUE-VAN ES  
Administrateur de Projets TPSGC

Drawing title  
**MECHANICAL  
PLAN & DETAILS**  
Titre du dessin

Project no./No. du projet  
R042523.006  
Drawing no./No. du dessin  
P-101  
Revision no.  
0

