

**PUBLIC WORKS AND GOVERNMENT
SERVICES CANADA**

**Royal Canadian Mounted Police
RCMP Hangar
190 Veteran's Way
Aircraft Fueling System Modifications**

**Prince Albert, Saskatchewan
Project No. R042523.006**

ISSUED FOR TENDER

DRAWING LIST

<u>GENERAL</u>	
<u>TITLE</u>	<u>COVER & DRAWING LIST</u>
G-101	LOCATION PLAN
G-102	GENERAL ARRANGEMENT

<u>CIVIL</u>	
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Public Works and
Government Services
Canada

Travaux publics et
Services gouvernementaux
Canada

**EGE Engineering Ltd.
Engineering,
Geosciences and
Environmental**



ASSOCIATED ENGINEERING QUALITY MANAGEMENT	QUALITY MANAGER <i>Richard</i>	DATE 2016-06-23	PROJECT CAD LEAD <i>R</i>	DATE 2016-03-22
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 NUMBER C116
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Revision	Description	Date
0	ISSUED FOR TENDER	6/15/16

Client: _____

Project title: **AIRCRAFT FUELING SYSTEM MODIFICATIONS
 RCMP HANGAR
 PRINCE ALBERT, SK**

Designed by: M. AKISTER
 Drawn by: N. MARKHAM
 Approved by: J. MEDORI
 PWGSC Project Manager: J. LA RUE-VAN ES

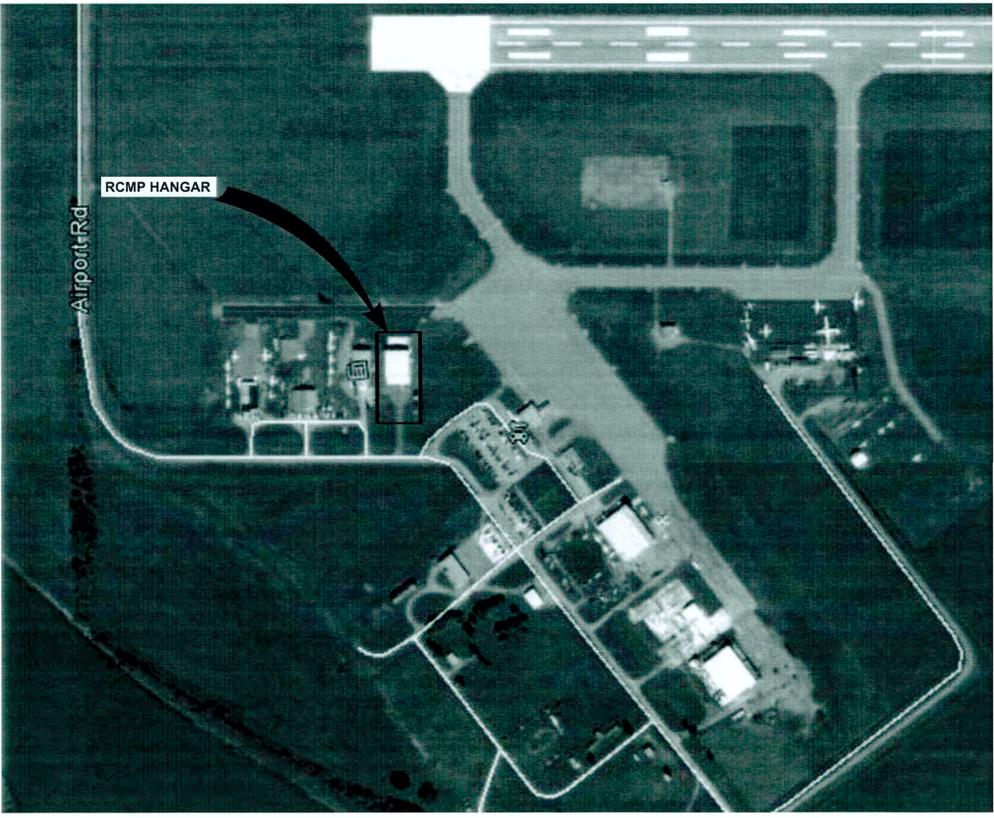
LOCATION PLAN

Project no./No. du projet: R042523.006	Drawing no./No. du dessin: G-101	Revision no.: 0
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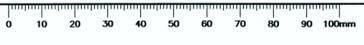
PRINCE ALBERT AIRPORT



1 PLAN NTS
 PRINCE ALBERT AIRPORT LOCATION PLAN



2 PLAN NTS
 PRINCE ALBERT RCMP HANGAR LOCATION PLAN





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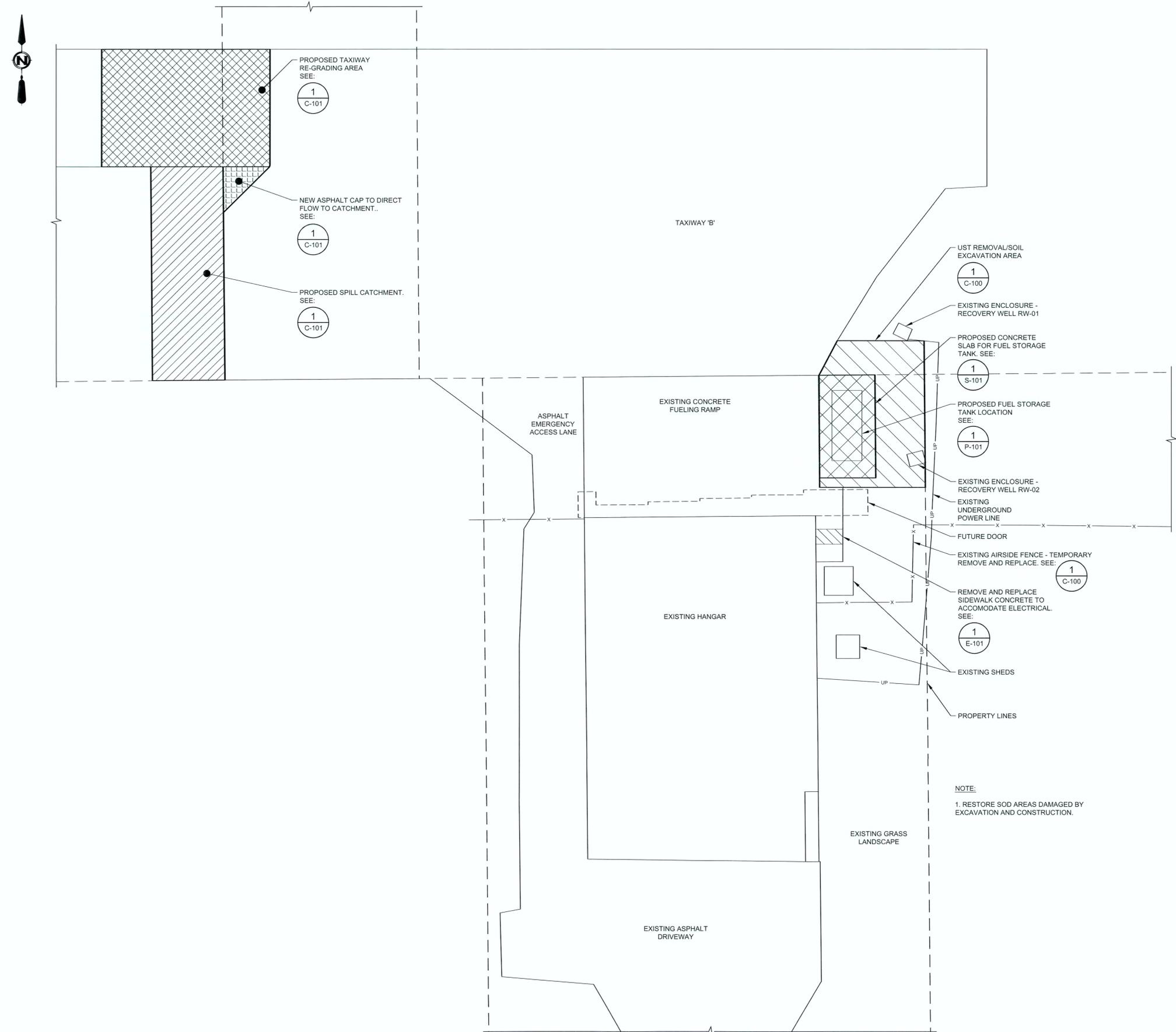
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Project title: **AIRCRAFT FUELING SYSTEM MODIFICATIONS RCMP HANGAR PRINCE ALBERT, SK**

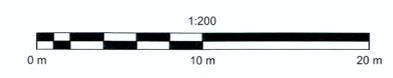
Designed by: M. AKISTER
 Drawn by: N. MARKHAM
 Approved by: J. MEDORI
 PWGSC Project Manager: J. LA RUE-VAN ES

GENERAL ARRANGEMENT

Project no./No. du projet	Drawing no./No. du dessin	Revision no.
R042523.006	G-102	0



1 PLAN SITE PLAN 1:200





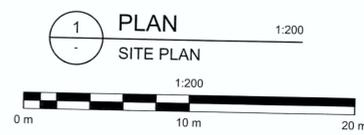
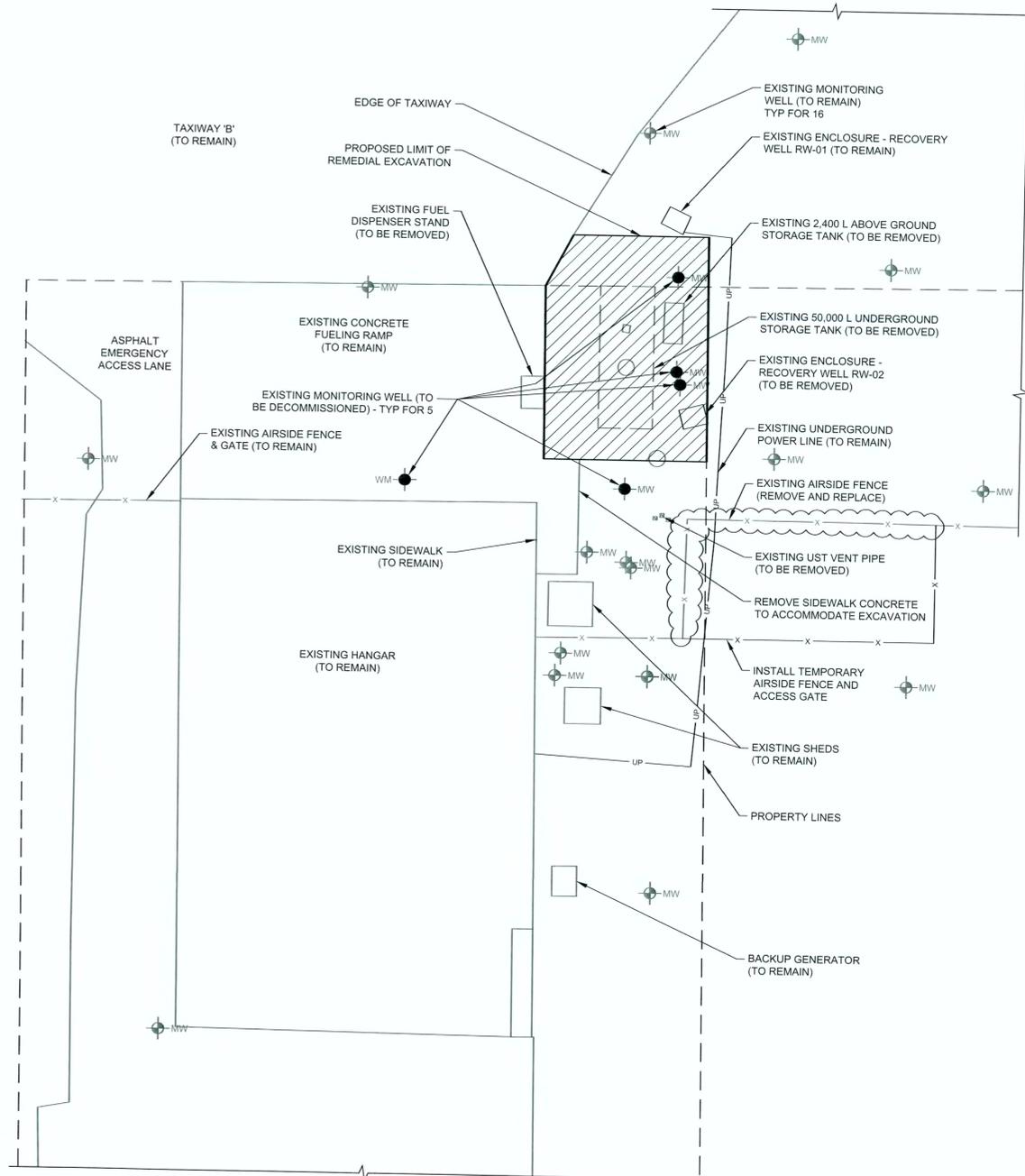
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Revision	Description	Date
0	ISSUED FOR TENDER	6/15/16

Project title: **AIRCRAFT FUELING SYSTEM MODIFICATIONS RCMP HANGAR PRINCE ALBERT, SK**

Designed by: A. PASSALIS (EGE) *Conçu par*
 Drawn by: N. MARKHAM *Dessiné par*
 Approved by: J. MEDORI *Approuvé par*
 PWSSC Project Manager: J. LA RUE-VAN ES *Administrateur de Projets TPSGC*

Drawing title: **EXCAVATION PLAN**
 Project no./No. du projet: R042523.006
 Drawing no./No. du dessin: C-101
 Revision no.: 0



- NOTES:**
- SHORING AND PROTECTION OF EXCAVATIONS ADJACENT PROPERTIES, AND EXISTING STRUCTURES TO PRE-CONSTRUCTION CONDITION REMAIN THE RESPONSIBILITY OF THE CONTRACTOR.
 - DESIGN AND PROVISION OF SHORING REMAINS THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE SHORING DESIGN ENGINEER IS RESPONSIBLE TO ENSURE THAT ALL EXISTING STRUCTURES AND PAVED SURFACES ARE TO REMAIN WITHOUT DISRUPTION OF SERVICE. CONTRACTOR TO MAINTAIN THE INTEGRITY OF ALL BUILDINGS AND PAVED SURFACES ONSITE TO PRE-CONSTRUCTION CONDITIONS THROUGH THE USE OF SHORING AND PROTECTION DURING EXCAVATION AND CONSTRUCTION. ENGINEER SHALL SUBMIT SHORING DETAIL DESIGN DRAWING(S) TO THE DEPARTMENTAL REPRESENTATIVE FOR REVIEW PRIOR TO PROCEEDING WITH THE WORK. THE DESIGN AND DRAWINGS SHALL BE SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE PROVINCE OF SASKATCHEWAN AND HOLDS A CURRENT "CERTIFICATION OF AUTHORIZATION" OF APEGS.
 - PROTECT ALL MONITORING WELLS NOT IDENTIFIED TO BE DECOMMISSIONED.



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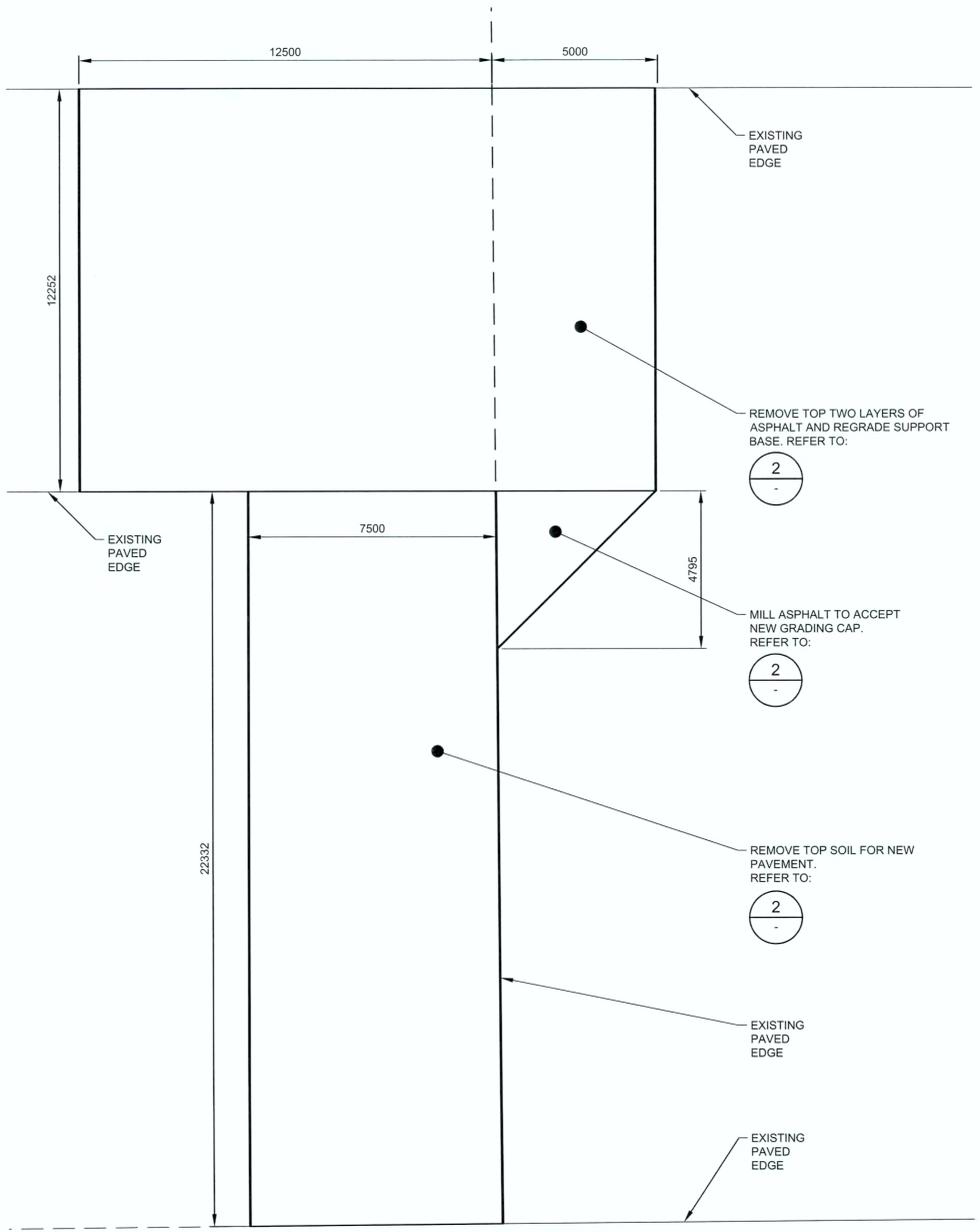
Revision	Description	Date
0	ISSUED FOR TENDER	6/15/16

Project title Projet
AIRCRAFT FUELING SYSTEM MODIFICATIONS
RCMP HANGAR
PRINCE ALBERT, SK

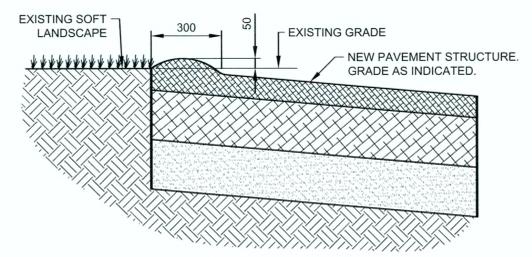
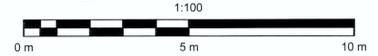
Designed by: M. AKISTER Conçu par
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 Approved by: J. MEDORI Approuvé par
 PWSC Project Manager: J. LA RUE-VAN ES Administrateur de Projets IPSGC

CIVIL
PLAN & DETAILS

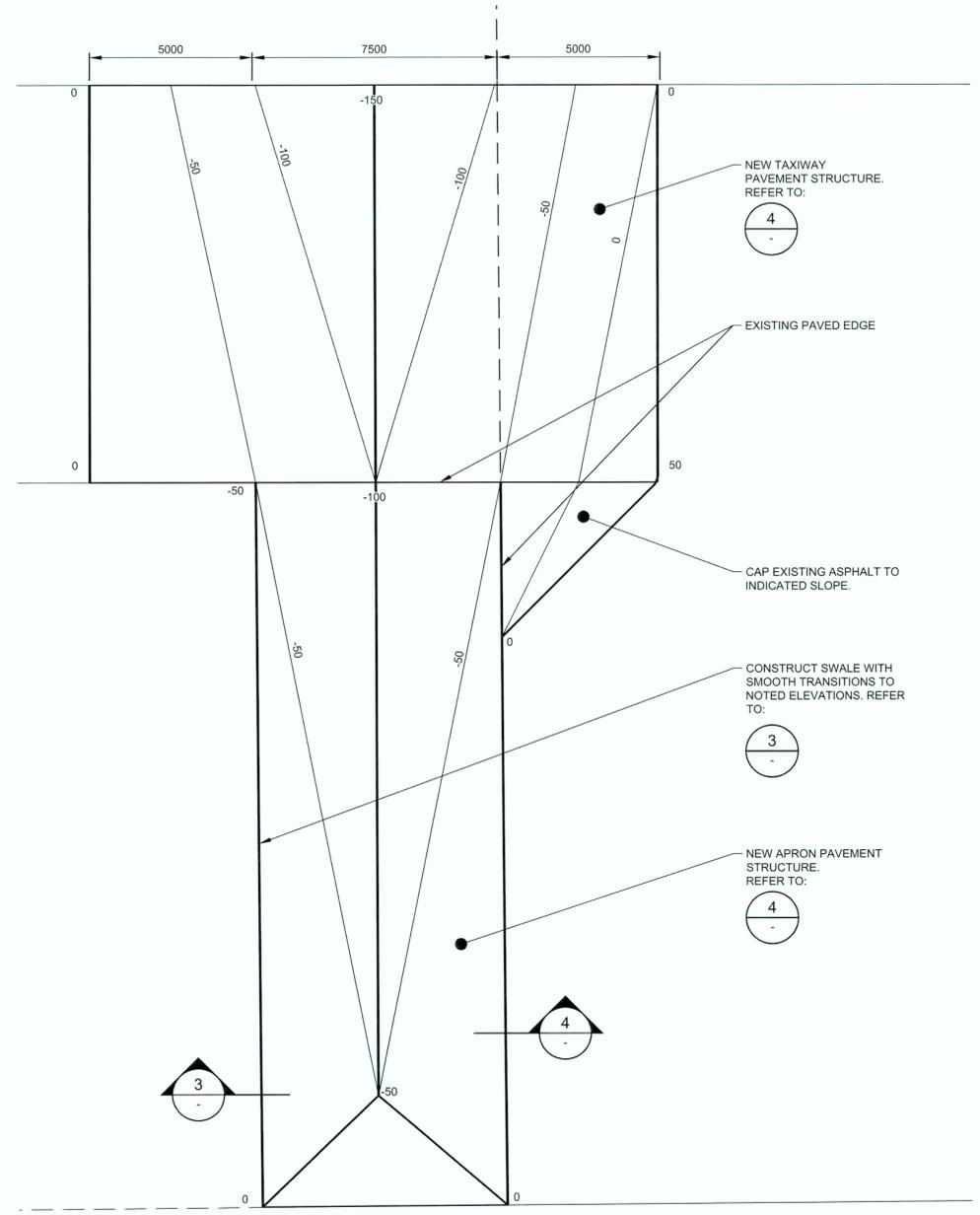
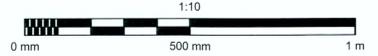
Project no./No. du projet	Drawing no./No. du dessin	Revision no.
R042523.006	C-102	0



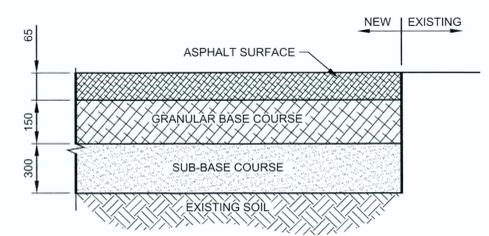
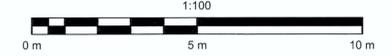
1 DETAIL 1:100
 SITE DEMOLITION PLAN



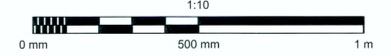
3 SECTION 1:10
 ASPHALT SWALE



2 DETAIL 1:100
 SITE PLAN

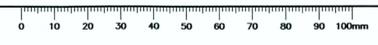


4 SECTION 1:10
 PAVEMENT STRUCTURE



NOTES:

- EXISTING ASPHALT BASE MATERIAL CAN BE SALVAGED FOR USE AS NEW SUPPORT BASE MATERIAL AS REVIEWED AND ACCEPTED IN WRITING BY THE DEPARTMENTAL REPRESENTATIVE
- ASPHALT CAP AREA TO BE ESTABLISHED IN FIELD BASED ON EXISTING ELEVATIONS
- SUBGRADE TO BE INSPECTED BY ENGINEER AFTER TOPSOIL STRIPPING AND PRIOR TO PLACEMENT OF GRAVEL MATERIAL. IF IRREGULARITIES SUCH AS ORGANICS OR SOFT SPOTS, THE EXISTING MATERIAL WILL BE REMOVED AND REPLACED WITH COMPETENT MATERIAL AS DIRECTED BY THE ENGINEER.
- SAW CUT EXISTING ASPHALT PAVEMENT AS DIRECTED BY ENGINEER TO PROVIDE SMOOTH SURFACE FOR BUTT JOINT.
- APPLY TACK COAT TO EDGE OF EXISTING ASPHALT PRIOR TO PLACING NEW ASPHALT
- THE TRANSITION FROM ONE SLOPE TO ANOTHER SLOPE SHOULD BE ACCOMPLISHED BY A CURVED SURFACE WITH A RATE OF CHANGE NOT EXCEEDING 5 mm IN A 4.5 m STRAIGHT EDGE
- REPAINT EXISTING 150 mm WIDE YELLOW MARKING ALONG EXISTING TAXIWAY CENTRELINE.





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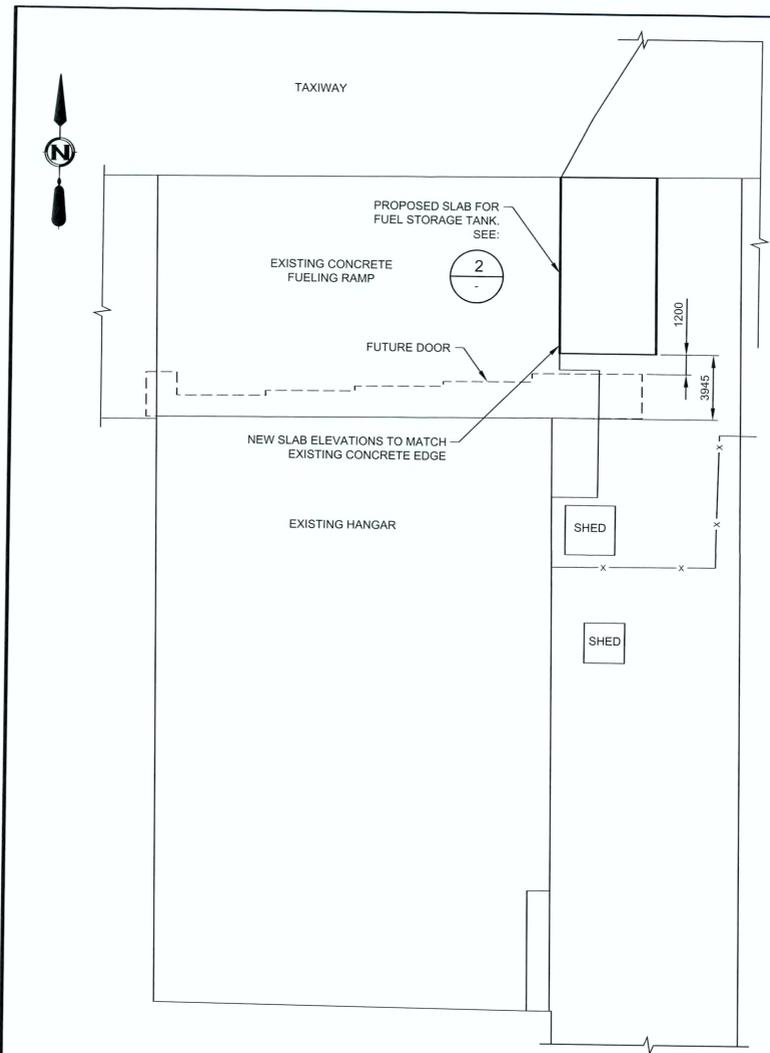
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Project title: **AIRCRAFT FUELING SYSTEM MODIFICATIONS RCMP HANGAR PRINCE ALBERT, SK**

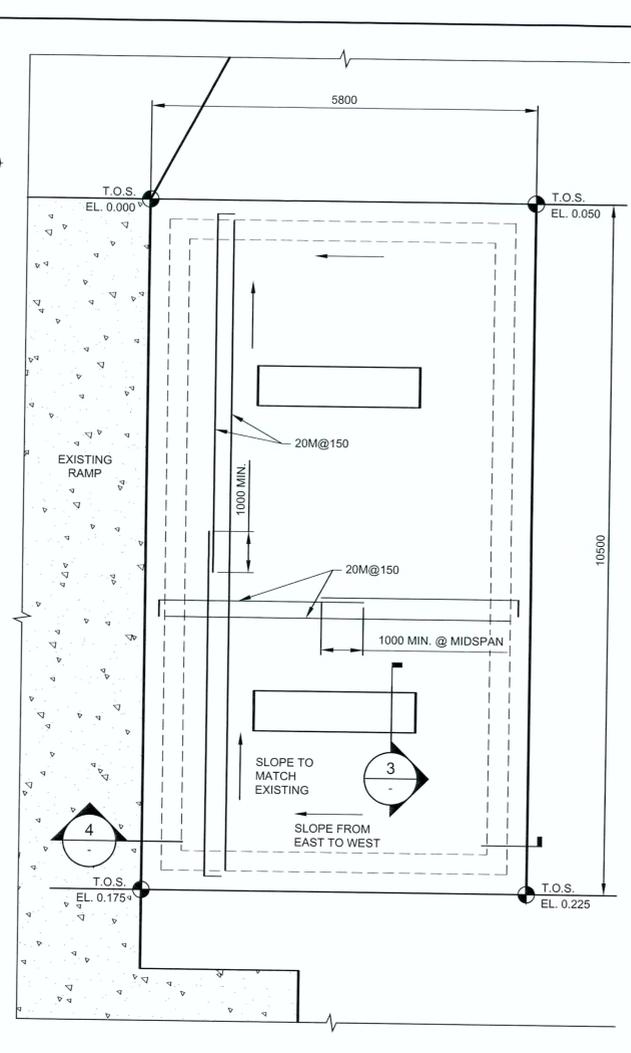
Designed by: S. CHIASSON
 Drawn by: N. MARKHAM
 Approved by: S. CHIASSON
 PWSSC Project Manager: J. LA RUE-VAN ES
 Conçu par: Dessiné par: Approuvé par: Administrateur de Projets TPSGC

STRUCTURAL PLAN & DETAILS

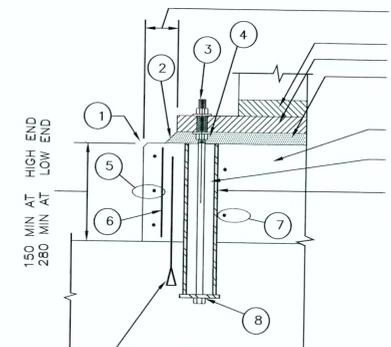
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R042523.006	S-101	0



1 PLAN
 ABOVE GROUND STORAGE TANK LOCATION
 1:200
 Scale bar: 0 m to 20 m



2 DETAIL
 STORAGE TANK SUPPORT SLAB
 1:50
 Scale bar: 0 m to 5 m



- 10M 'HILT' ANCHORS @ 200 O.C. C/W 250 L.G. TREADED ROD (MIN. 3 EACH SIDE)**
- 1 CHAMFER ALL BASE EDGES
 - 2 45° FINISHING EDGE ON GROUT BED
 - 3 'HILT' ANCHOR BOLT C/W NUT & WASHER
 - 4 JACKING NUT C/W WASHER (OR SHIMS)
 - 5 15M-TIES @ 200
 - 6 15M@200
 - 7 2-10M INSIDE BOLTS
 - 8 TACK WELD BOLT TO PREVENT TURNING

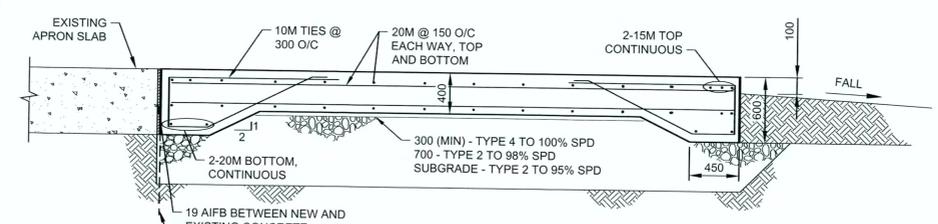
3 SECTION
 TANK SUPPORT PIER
 1:10
 Scale bar: 0 mm to 1 m

100 mm MIN. ALL AROUND OR AS SPECIFIED
 BASE SUPPORT SADDLE (BY SUPPLIER)
 SOLE PLATE (BY SUPPLIER)
 25mm MIN. OR AS SPECIFIED, NON SHRINK, NON METALLIC GROUT - SEE SPECIFICATIONS
 TANK SUPPORT PIER
 FILL ANCHOR BOLT SLEEVE WITH EPOXY GROUT PRIOR TO FINAL SOLEPLATE INSTALL.
 ANCHOR BOLT SLEEVE, FABRICATE FROM STD. WT. STEEL PIPE, MIN. 40 mm LARGER THAN BOLT DIAMETER

- NOTES:**
- ANCHOR BOLT, NUTS AND WASHERS - SIZE, NUMBER REQUIRED, THREAD PROJECTION AND LENGTH TO SUIT TANK. MANUFACTURERS SPECIFICATIONS. 150mm MIN FLOOR PENETRATION.
 - JACKING NUTS MAY BE REPLACED WITH LEVELING SHIMS. SHIMS ARE TO REMAIN IN PLACE, THEN PLACE GROUT.
 - FOUNDATION BOLTS TO BE GR. B8 OR B8M S.S. AND SHALL BE AT LEAST EQUAL IN SIZE TO PUMP DISCH HEAD BOLTS.
 - FOUNDATION BOLTS MUST BE INSIDE OF REINF. BARS PERIMETER.
 - SIZE AND LOCATION OF PIERS TO BE COORDINATED WITH MECHANICAL.

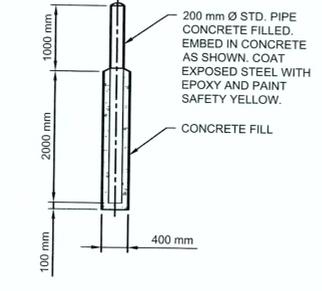
GENERAL NOTES

- A. COORDINATION**
- ALL WORKMANSHIP, COMPONENT DESIGN AND MATERIAL SHALL BE TO THE NATIONAL BUILDING CODE OF CANADA - NBCC2010 OR BETTER.
 - CHECK ALL DIMENSIONS, ELEVATIONS AND DETAILS PRIOR TO CONSTRUCTION OR FABRICATION. REPORT ANY DISCREPANCIES OR DESIRED MODIFICATIONS TO THE DEPARTMENTAL REPRESENTATIVE.
 - FOR ATTACHMENT AND LEVELLING OF TANK, THE ANCHOR BOLTS, NUTS AND WASHERS - SIZE, NUMBER REQUIRED, THREAD PROJECTION AND LENGTH TO SUIT TANK MANUFACTURERS SPECIFICATIONS.
 - SLEEVE TYPE ANCHOR BOLT MAY BE REPLACED WITH J-BOLT TYPE ANCHOR OR EPOXY ADHESIVE ANCHOR BOLT IF APPROVED BY DEPARTMENTAL REPRESENTATIVE AND PUMP SUPPLIER.
- B. FOUNDATIONS**
- FOUNDATION DESIGN IS BASED UPON THE FOUNDATION INVESTIGATION SOILS REPORT PREPARED BY P. MACHIBRODA ENGINEERING LTD, TITLED "GEOTECHNICAL INVESTIGATION PROPOSED SHORED EXCAVATION, RCMP HANGAR - PRINCE ALBERT AIRPORT, 190 VETERANS WAY, PRINCE ALBERT, SASKATCHEWAN, PMEL FILE NO. 10121" DATED NOVEMBER 30, 2015. ENSURE THAT THE REQUIREMENTS OUTLINED IN THIS REPORT ARE READ & UNDERSTOOD PRIOR TO COMMENCING WITH FOUNDATION WORK.
 - a. SOIL CONDITIONS ARE ASSUMED TO HAVE AN ALLOWABLE BEARING PRESSURE = 50 kPa.
 - b. UNDER NO CIRCUMSTANCE IS THE SOIL UNDER THE STRUCTURE TO BE ALLOWED TO FREEZE, DRY OUT OR BECOME SATURATED PRIOR TO, DURING OR SUBSEQUENT TO CONSTRUCTION.
- C. EXCAVATION AND BACKFILL.**
- EXCAVATE AND BACKFILL TO THE DETAILS ON THIS DRAWING, REFER TO SPECIFICATIONS FOR REQUIREMENTS.
- D. REINFORCED CONCRETE**
- REINFORCING STEEL: GRADE 400 DEFORMED BARS. HOOK BARS AT OPPOSITE FACE AT DISCONTINUOUS ENDS. PROVIDE CLASS 'B' LAP SPLICES THROUGHOUT EXCEPT WHERE OTHER DIMENSIONS ARE SHOWN. TIE AND SECURE IN PLACE PRIOR TO PLACING CONCRETE. WHERE REINFORCING IS SHOWN IN ONE DIRECTION ONLY, PROVIDE 15M @ 250 O/C EACH FACE AND PERPENDICULAR TO THAT SHOWN.
 - CONCRETE COVER TO REINF. STEEL:
 - 75 mm FOR CONCRETE CAST AGAINST SOIL.
 - 50 mm FOR CONCRETE CAST AGAINST ALL OTHER SURFACES UNLESS SPECIFICALLY NOTED.
 - ENSURE ALL REINFORCING STEEL IS 50 mm CLEAR OF ANY EMBEDDED ITEMS.
 - ACCURATELY PLACE REINFORCEMENT AND SECURE AGAINST DISPLACEMENT DURING CONCRETE PLACING. SUPPORT BY MEANS OF EPOXY COATED OR PLASTIC CHAIRS AND SPACERS, AND THE RECOMMENDATIONS OF "REINFORCING STEEL - MANUAL OF STANDARD PRACTICE" (REINFORCING STEEL INSTITUTE OF CANADA).
 - USE TOP BAR CLASS 'B' TENSION SPLICES UNLESS NOTED OTHERWISE.
 - CONCRETE MIXES SHALL BE PROPORTIONED IN ACCORDANCE WITH CSA A23.1. SEE SPECIFICATIONS FOR DETAILED REQUIREMENTS.
 - PROVIDE 20 mm CHAMFER AT ALL EXPOSED CONCRETE EDGES UNLESS NOTED OTHERWISE.
 - CONCRETE MIXES SHALL BE PROPORTIONED IN ACCORDANCE WITH CSA A23.1/a23.2 TO MEET THE FOLLOWING REQUIREMENTS:
 - a. EXPOSURE CLASS: s-1
 - b. -56 DAY COMPRESSIVE STRENGTH: 35 MPa
 - c. -MAX W/CM RATIO: 0.4
 - d. -NOMINAL AGGREGATE SIZE: 20 mm
 - e. -MAX SLUMP: 120 mm - ON SITE SLUMP TEST TO BE DONE BY 3RD PARTY HIRED BY CONTRACTOR.
 - CONCRETE SLAB FINISHED BY FLOATING FOLLOWED BY TROWELING.
 - SANDBLAST, CLEAN AND ROUGHEN ALL EXISTING CONCRETE AND CONSTRUCTION JOINTS TO A FULL 5 mm AMPLITUDE PRIOR TO POURING NEW CONCRETE. APPLY EPOXY BASED BONDING AGENT PRIOR TO POUR.
 - MAKE AND TEST FOUR CYLINDERS FOR EACH CONCRETE PLACEMENT (1@7 DAYS, 3@28 DAYS).
 - HONEYCOMB OR EMBEDDED DEBRIS IN CONCRETE IS NOT ACCEPTABLE. NOTIFY THE DEPARTMENTAL REPRESENTATIVE UPON DISCOVERY. REMOVE AND REPLACE DEFECTIVE CONCRETE AS DIRECTED BY THE DEPARTMENTAL REPRESENTATIVE.
 - VIBRATE FRESH CONCRETE AND/OR FORM WORK ADEQUATELY TO PRODUCE SOUND CONCRETE WITHOUT HONEYCOMBS.



4 SECTION
 STORAGE TANK SUPPORT SLAB
 1:20
 Scale bar: 0 m to 2 m

- NOTES:**
- TANK SUPPORT PIER DIMENSIONS AND LOCATIONS TO BE COORDINATED WITH MECHANICAL
 - TANK SUPPORT PIER HEIGHT TO BE SET TO NEUTRALIZE SLAB SLOPE
 - SET WEST AND SOUTH SLAB
 - ELEVATIONS TO MATCH EXISTING CONCRETE ELEVATIONS



5 DETAIL
 BOLLARD
 SCALE: N.T.S.
 P-101





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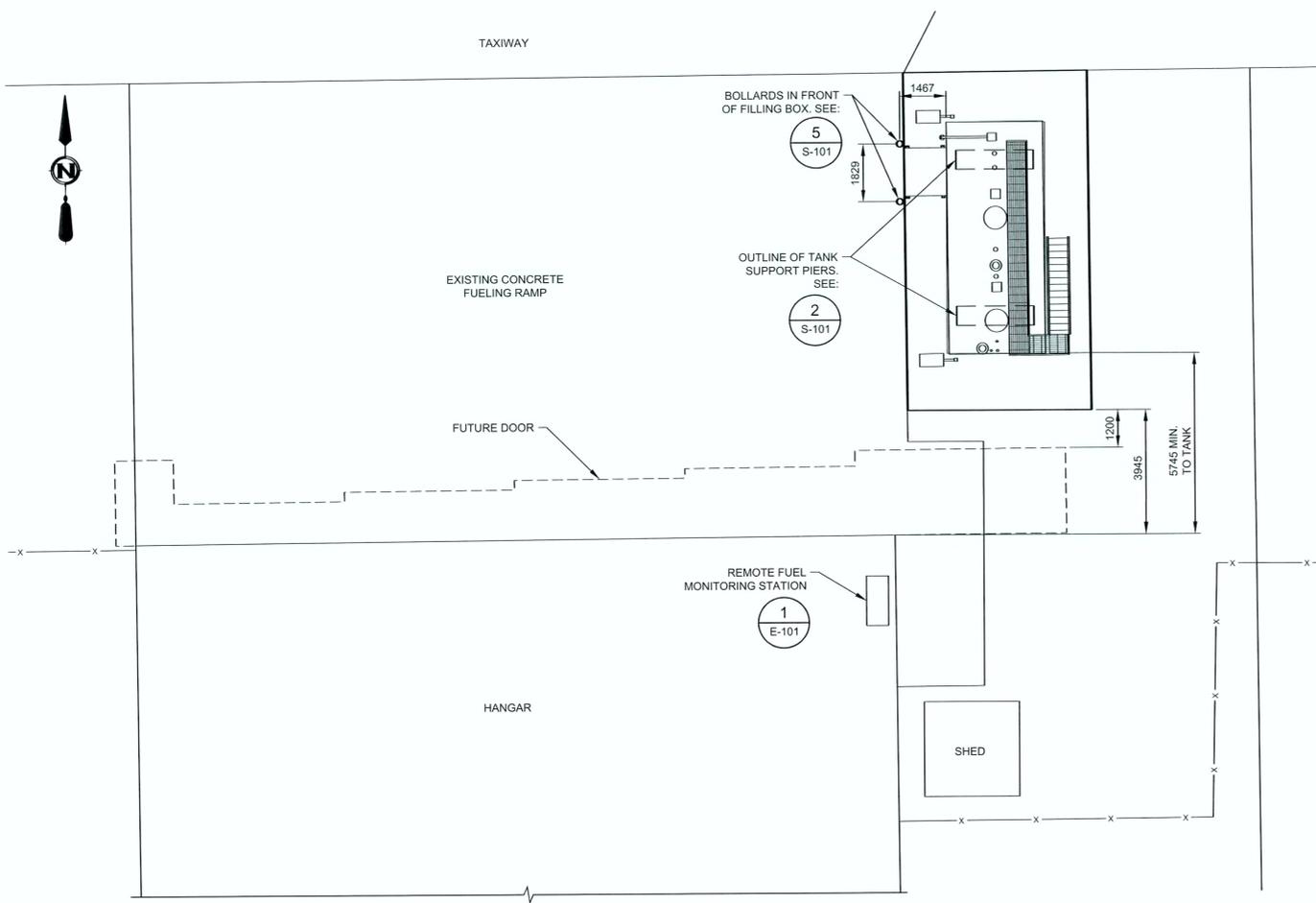
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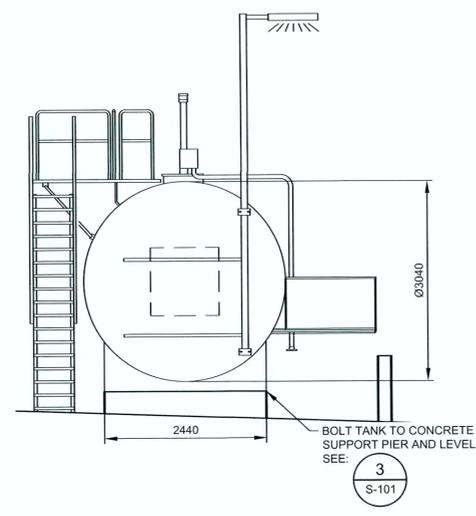
Designed by: M. AKISTER
 Drawn by: N. MARKHAM
 Approved by: M. AKISTER

PWOSC Project Manager / Administrateur de Projets TPSGC: J. LA RUE-VAN ES
 Drawing title: **MECHANICAL PLAN & DETAILS**

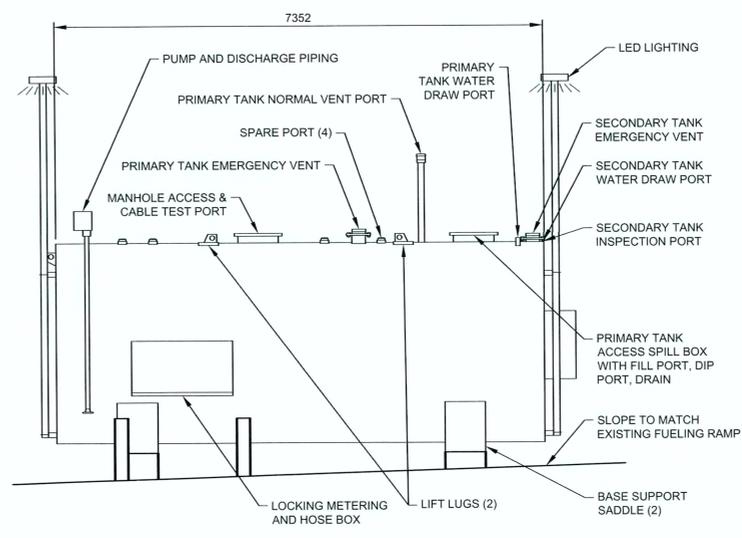
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R042523.006	P-101	0



1 PLAN
 ABOVE GROUND STORAGE TANK
 1:100
 Scale bar: 0 m, 5 m, 10 m

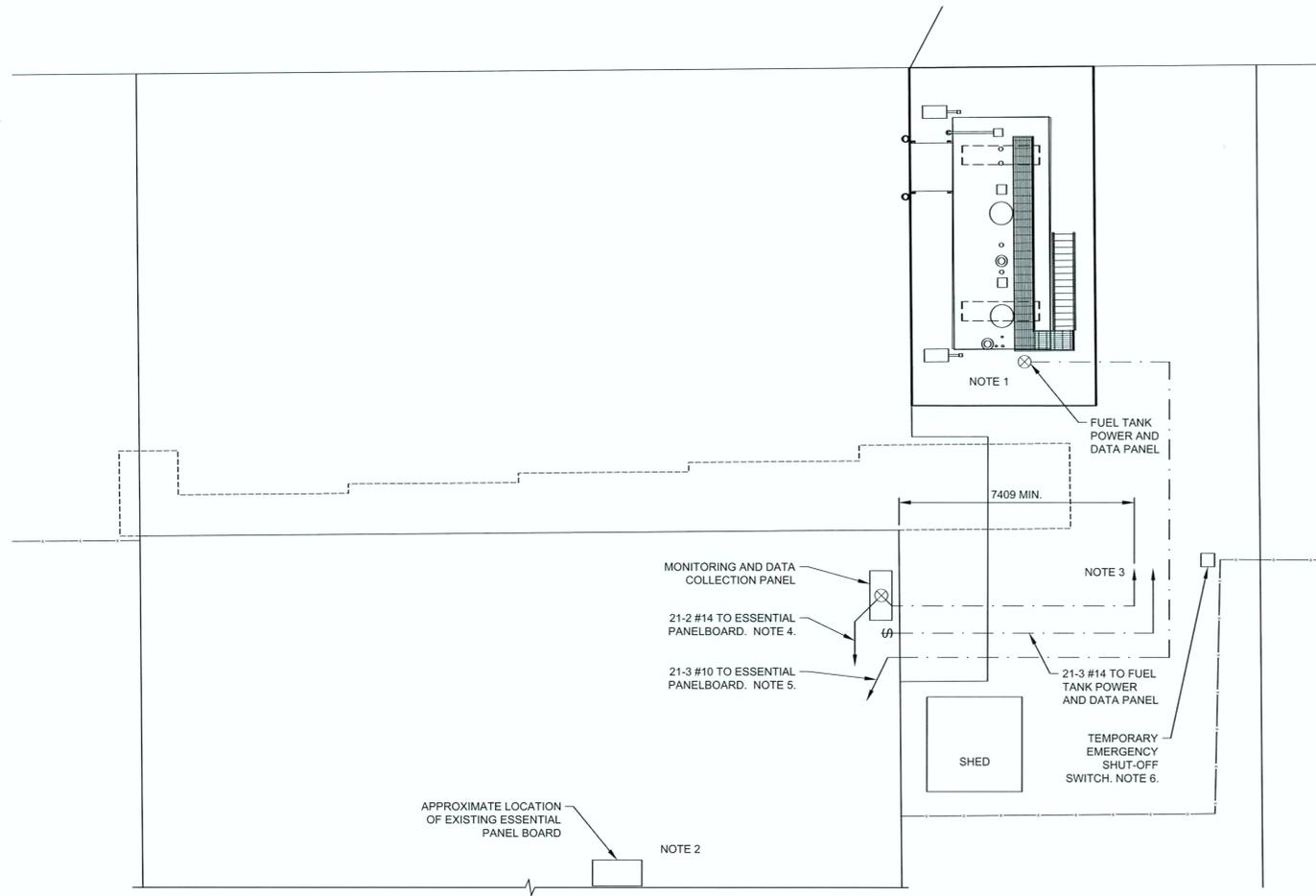


2 ELEVATION
 ABOVE GROUND STORAGE TANK SOUTH ELEVATION
 1:50
 Scale bar: 0 m, 1 m, 5 m

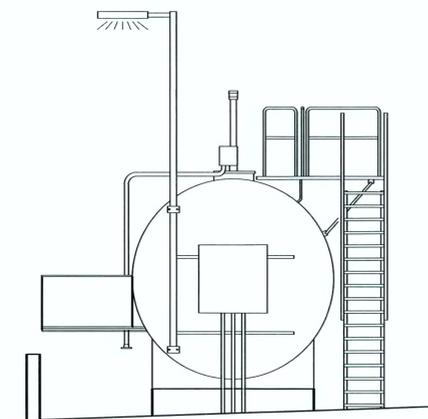


3 ELEVATION
 ABOVE GROUND STORAGE TANK WEST ELEVATION
 1:50
 Scale bar: 0 m, 1 m, 5 m

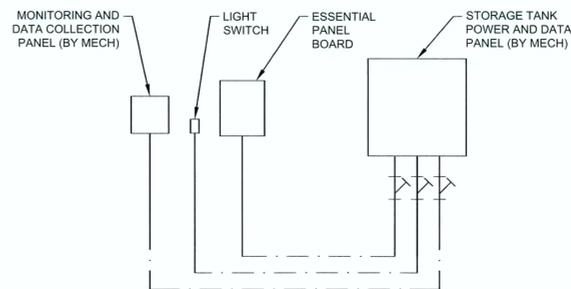
- NOTES:**
- ABOVE GROUND STORAGE TANK TO INCLUDE THE FOLLOWING:
 - 45,000 L CAPACITY FOR JET "A" FUEL
 - COMPLY WITH UL-C-S601
 - 100% SECONDARY CONTAINMENT
 - 30m HOSE WITH MOTORIZED REEL AND DISPENSING METER IN LOCKING CABINET
 - ACCEPTABLE HOSES: API BULL 1529 TYPE C, F, OR CT
 - DUAL 2 STAGE FILTRATION WITH AUTOMATIC SWITCHOVER
 - ACCESS STAIRS AND INSPECTION PLATFORM TO PROVIDE ACCESS TO ALL TOP OPENINGS
 - CONTINUOUS PRODUCT LEVEL AND LEAK DETECTION MONITORING SYSTEMS, INCLUDING AUDIBLE AND VISUAL ALARMS, DISPENSING VOLUME RECORDING LOGS & REMOTE CONTROL CONNECTION PROVISIONS.
 - ON TANK LIGHTING
 - GRAVITY FILLING WITH SPILL BOX FOR TANK TRUCKS
 - SECURITY AGAINST THEFT AND VANDALISM
 - FACTORY PRE-WIRED
 - WEATHER PROOF CABINET FOR ELECTRICAL CONNECTIONS AND DISCONNECTS
 - TURN OVER STATUS MONITORING AND DISPENSING RECORDER TO ELECTRICAL TRADE FOR INSTALLATION INSIDE HANGAR
 - COORDINATE CONCRETE PIER DIMENSIONS AND ANCHOR BOLT PLACEMENT WITH STRUCTURAL
 - BOLT TANK BASE SUPPORT TO CONCRETE PIER. LEVEL AND GROUT.
 - COORDINATE SIZE AND LOCATION OF EACH PIER WITH STRUCTURAL TO MINIMIZE GROUTING REQUIREMENTS
 - PAD ELEVATION TO MATCH FUELING RAMP



1 PLAN 1:100
 ABOVE GROUND STORAGE TANK
 1:100
 0 m 5 m 10 m



2 ELEVATION 1:50
 ABOVE GROUND STORAGE TANK NORTH ELEVATION
 1:50
 0 m 1 m 5 m



3 SCHEMATIC NTS
 STORAGE TANK CONNECTIONS

- ELECTRICAL LEGEND**
- ⊗ EQUIPMENT CONNECTION
 - SURFACE CONDUIT
 - - - BURIED CONDUIT
 - \$ SPST SWITCH
 - ⌞ EYS FITTING

- NOTES:**
1. ROUGH IN CONDUITS IN SLAB. COORDINATE LOCATION WITH STRUCTURAL AND MECHANICAL.
 2. USE EXISTING 3P-30A FUEL DISPENSING BREAKER IN ESSENTIAL PANELBOARD TO FEED NEW FUEL TANK
 3. PROVIDE CONDUIT SEALS ON CONNECTIONS TO FUEL TANK
 4. ROUTE NEW CONDUIT IN HARMONY WITH EXISTING
 5. SALVAGE OF EXISTING FUEL DISPENSING FEEDER INSIDE THE HANGAR IS ACCEPTABLE
 6. STORAGE TANK EMERGENCY SHUT-OFF SWITCH AND LABELING SHALL BE RELOCATED AFTER HANGAR DOOR MODIFICATION. PERMANENT LOCATION TO BE CONFIRMED ON-SITE WITH OWNER.
 7. HAZARDOUS LOCATION AREAS ARE TO BE PER CANADIAN ELECTRICAL CODE AND SASKATCHEWAN INTERPRETATIONS



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Project title: **AIRCRAFT FUELING SYSTEM MODIFICATIONS RCMP HANGAR PRINCE ALBERT, SK**

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Drawing title: **ELECTRICAL DETAILS**

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