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**SOLICITATION AMENDMENT**  
**MODIFICATION DE L'INVITATION**

The referenced document is hereby revised; unless otherwise indicated, all other terms and conditions of the Solicitation remain the same.

Ce document est par la présente révisé; sauf indication contraire, les modalités de l'invitation demeurent les mêmes.

Comments - Commentaires

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<b>Title - Sujet</b> Aircraft Training / Maintenance Pla	
<b>Solicitation No. - N° de l'invitation</b> W0125-17LM01/A	<b>Amendment No. - N° modif.</b> 007
<b>Client Reference No. - N° de référence du client</b> W0125-17-LM01	<b>Date</b> 2016-08-05
<b>GETS Reference No. - N° de référence de SEAG</b> PW-\$KIN-930-6947	
<b>File No. - N° de dossier</b> KIN-6-46022 (930)	<b>CCC No./N° CCC - FMS No./N° VME</b>
<b>Solicitation Closes - L'invitation prend fin</b> <b>at - à 02:00 PM</b> <b>on - le 2016-08-16</b>	
<b>F.O.B. - F.A.B. Specified Herein - Précisé dans les présentes</b>	
<b>Plant-Usine:</b> <input type="checkbox"/> <b>Destination:</b> <input type="checkbox"/> <b>Other-Autre:</b> <input checked="" type="checkbox"/>	
<b>Address Enquiries to: - Adresser toutes questions à:</b> Littlefield, Mike	<b>Buyer Id - Id de l'acheteur</b> kin930
<b>Telephone No. - N° de téléphone</b> (613) 545-8058 ( )	<b>FAX No. - N° de FAX</b> (613) 545-8067
<b>Destination - of Goods, Services, and Construction:</b> <b>Destination - des biens, services et construction:</b>	

Instructions: See Herein

Instructions: Voir aux présentes

<b>Delivery Required - Livraison exigée</b>	<b>Delivery Offered - Livraison proposée</b>
<b>Vendor/Firm Name and Address</b> <b>Raison sociale et adresse du fournisseur/de l'entrepreneur</b>	
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<b>Name and title of person authorized to sign on behalf of Vendor/Firm</b> <b>(type or print)</b> <b>Nom et titre de la personne autorisée à signer au nom du fournisseur/</b> <b>de l'entrepreneur (taper ou écrire en caractères d'imprimerie)</b>	
<b>Signature</b>	<b>Date</b>

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Aircraft Training and Maintenance Platforms

Amendment 007 is being issued in response to Bidder's questions and Canada's responses and to amend Annex "A" Statement of Work.

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**Q1.** Reference Annex A Statement of Work Section 3.2.1 (f)

***In Amendment 004 Question #5, it was asked "Please identify areas where stair access is required." The answer was given - There is no specified area that stair access is required. It would seem important to state a minimum of access points surrounding the training platform. The quantity of such could make a significant different in bid price. Please provide a minimum stair quantity required.***

**A1.** At minimum, stairway access is required for accessing the R/H and L/H areas of the trainer. As the stands require a continuous walkway/working area around the trainer and with the exception of existing stands around the L/H outboard engine, stairway access is to ensure no interference with existing stands at Outboard engine, trench areas or aircraft access panels and doors.

**Refer:** 3.2.1 (f) Annex "A" Statement of Work

**Delete:** in its entirety.

**Insert:** At a minimum, two sets of stairways are required for accessing the aircraft maintenance platform. Once to access the right hand side of the C-130 trainer and the other the left hand side. Stairs are required as opposed to ladders;

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**Q2.** Reference Annex A Statement of Work Section 3.2.1 (h)

***Reference is made to Amendment 004, Question #16. Please provide OWS number location for this jack.***

**A2.** The OWS for the Wing Fuselage Jack is CWS 216.000.

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**Q3.** Reference is made to Amendment 004, Question #1. Please provide OWS number for this jack (see below). This will help with having a clearer understanding of necessary platform coverage.

**A3.** The pictured jack (Wing Steady) is located at OWS 283.000



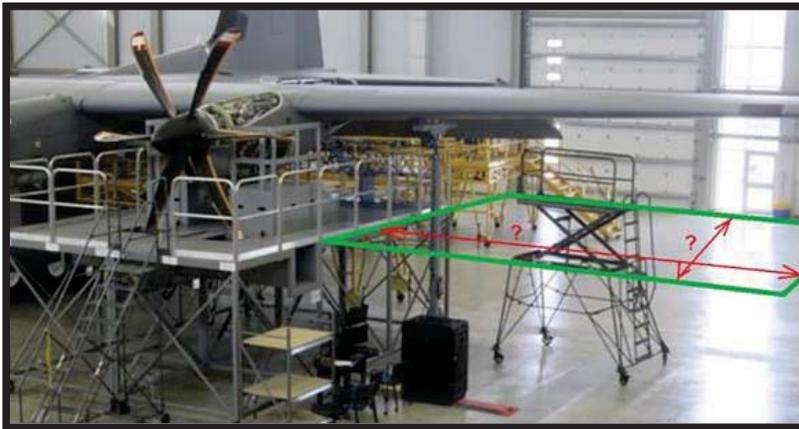
**Q4. Reference Annex A Statement of Work Section 3.2.4 (g)**

***Confused on what this is referring to? Is this a stand and its required coverage, or is a space required? Please clarify this.***

**A4.** This referring to an area of coverage of the maintenance stand to account for and allow technicians to place tools/panels/parts etc. while conducting practical training exercises on the maintenance stands.

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**Q5. If the above is not a stand, what exactly is the required platform coverage area overboard of the fixed wing jack (see photo below)? How much forward of the wing in this area and how much aft? And does the platform need to extend from the edge of existing to 5ft past the wing tip?**



**A5.** The question above does in fact refer to a stand. Requirement is that stand must extend 8 ft./2.4384 metres forward of leading edge and 8 ft./2.4384 aft of trailing edge. The platform(s) must extend from edge of existing stands to 5 ft./1.524 metres past wing tip on L/H wing and platforms must extend 5 ft./1.524 metres past R/H wingtip.

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**Q6. Reference Annex A Statement of Work Section 3.3.3 (e)**

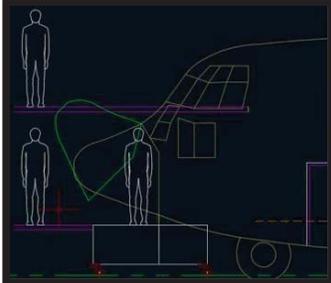
***Reference is made to Amendment 004, Question #10. Please explain the need and use of the POL system. Is the engine at this location solely used for install/removal training? If so, what is the intent of a drain pan? Does this need to be integrated in the stand or is this a separate pan that lies on top of the deck?***

**A6.** The need and use of the POL system is to minimize lubricants from spilling to areas below maintenance stands. The drain pan is to be placed under the engine area. No specification to integrate into stand but area underneath Inboard engine must incorporate a POL system (drain pan or similar type system).

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**Q7. Reference Annex A Statement of Work Section 3.2.2**

***Does the nose stand need to be two levels (see below)? Or are the lower training areas around the nose low enough to use the hanger floor to access those points?***



**A7.** The nose stand does not need to be two levels. Lower training areas around the nose are low enough to use hanger floor for access to nose land gear training areas. Picture in Amendment 4 shows small stair platform that is used to access areas in nose area.

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**ALL OTHER TERMS AND CONDITIONS REMAIN UNCHANGED**