



**RETURN BIDS TO:  
RETOURNER LES SOUMISSIONS À:**

**Bid Receiving - PWGSC / Réception des  
soumissions - TPSGC**

**11 Laurier St. / 11, rue Laurier**

**Place du Portage , Phase III**

**Core 0B2 / Noyau 0B2**

**Gatineau**

**Québec**

**K1A 0S5**

**Bid Fax: (819) 997-9776**

**REQUEST FOR PROPOSAL  
DEMANDE DE PROPOSITION**

**Proposal To: Public Works and Government  
Services Canada**

We hereby offer to sell to Her Majesty the Queen in right of Canada, in accordance with the terms and conditions set out herein, referred to herein or attached hereto, the goods, services, and construction listed herein and on any attached sheets at the price(s) set out therefor.

**Proposition aux: Travaux Publics et Services  
Gouvernementaux Canada**

Nous offrons par la présente de vendre à Sa Majesté la Reine du chef du Canada, aux conditions énoncées ou incluses par référence dans la présente et aux annexes ci-jointes, les biens, services et construction énumérés ici sur toute feuille ci-annexée, au(x) prix indiqué(s).

**Comments - Commentaires**

**Vendor/Firm Name and Address**

**Raison sociale et adresse du**

**fournisseur/de l'entrepreneur**

**Issuing Office - Bureau de distribution**

Marine Machinery and Services / Machineries et services maritimes

11 Laurier St. / 11, rue Laurier

6C2, Place du Portage

Gatineau

Québec

K1A 0S5

<b>Title - Sujet</b> Bow Windlass Parts (GSM)	
<b>Solicitation No. - N° de l'invitation</b> F2599-175002/A	<b>Date</b> 2017-02-15
<b>Client Reference No. - N° de référence du client</b> F2599-175002	
<b>GETS Reference No. - N° de référence de SEAG</b> PW-\$\$ML-055-26187	
<b>File No. - N° de dossier</b> 055ml.F2599-175002	<b>CCC No./N° CCC - FMS No./N° VME</b>
<b>Solicitation Closes - L'invitation prend fin</b> <b>at - à 02:00 PM</b> <b>on - le 2017-03-28</b>	<b>Time Zone</b> <b>Fuseau horaire</b> Eastern Standard Time EST
<b>F.O.B. - F.A.B.</b> <b>Plant-Usine:</b> <input type="checkbox"/> <b>Destination:</b> <input checked="" type="checkbox"/> <b>Other-Autre:</b> <input type="checkbox"/>	
<b>Address Enquiries to: - Adresser toutes questions à:</b> Belcaid, Sidi	<b>Buyer Id - Id de l'acheteur</b> 055ml
<b>Telephone No. - N° de téléphone</b> (819) 420-2292 ( )	<b>FAX No. - N° de FAX</b> ( ) -
<b>Destination - of Goods, Services, and Construction:</b> <b>Destination - des biens, services et construction:</b> DEPARTMENT OF FISHERIES AND OCEANS CCGS GRIFFON 401 KING ST W. Attenntion Chief Engineer PRESCOTT Ontario K0E1T0 Canada	

**Instructions: See Herein**

**Instructions: Voir aux présentes**

<b>Delivery Required - Livraison exigée</b> See Herein	<b>Delivery Offered - Livraison proposée</b>
<b>Vendor/Firm Name and Address</b> <b>Raison sociale et adresse du fournisseur/de l'entrepreneur</b>	
<b>Telephone No. - N° de téléphone</b> <b>Facsimile No. - N° de télécopieur</b>	
<b>Name and title of person authorized to sign on behalf of Vendor/Firm</b> <b>(type or print)</b> <b>Nom et titre de la personne autorisée à signer au nom du fournisseur/</b> <b>de l'entrepreneur (taper ou écrire en caractères d'imprimerie)</b>	
<b>Signature</b>	<b>Date</b>

## TABLE OF CONTENTS

<b>PART 1 - GENERAL INFORMATION .....</b>	<b>3</b>
1.1 SECURITY REQUIREMENTS .....	3
1.2 STATEMENT OF REQUIREMENT .....	3
1.4 SET-ASIDE UNDER THE FEDERAL GOVERNMENT PROCUREMENT STRATEGY FOR ABORIGINAL BUSINESS (PSAB) – N/A .....	3
1.5 DEBRIEFINGS .....	3
1.6 NATIONAL SECURITY EXCEPTION – N/A .....	3
1.7 TRADE AGREEMENTS .....	3
1.8 CANADIAN CONTENT – N/A .....	3
<b>PART 2 - BIDDER INSTRUCTIONS .....</b>	<b>4</b>
2.1 STANDARD INSTRUCTIONS, CLAUSES AND CONDITIONS .....	4
2.2 SUBMISSION OF BIDS .....	4
2.3 FORMER PUBLIC SERVANT – N/A .....	4
2.4 ENQUIRIES - BID SOLICITATION .....	4
2.5 APPLICABLE LAWS .....	5
2.6 OPTIONAL SITE VISIT – VESSEL .....	5
<b>PART 3 - BID PREPARATION INSTRUCTIONS .....</b>	<b>6</b>
3.1 BID PREPARATION INSTRUCTIONS .....	6
<b>PART 4 - EVALUATION PROCEDURES AND BASIS OF SELECTION .....</b>	<b>8</b>
4.1 EVALUATION PROCEDURES .....	8
4.2 BASIS OF SELECTION .....	8
<b>PART 5 – CERTIFICATIONS AND ADDITIONAL INFORMATION .....</b>	<b>9</b>
5.1 CERTIFICATIONS REQUIRED WITH THE BID .....	9
5.2 CERTIFICATIONS PRECEDENT TO CONTRACT AWARD AND ADDITIONAL INFORMATION .....	9
<b>PART 6 - RESULTING CONTRACT CLAUSES .....</b>	<b>11</b>
6.1 SECURITY REQUIREMENTS .....	11
6.2 STATEMENT OF REQUIREMENT .....	11
6.3 STANDARD CLAUSES AND CONDITIONS .....	11
6.4 TERM OF CONTRACT .....	11
6.5 AUTHORITIES .....	11
6.6 PROACTIVE DISCLOSURE OF CONTRACTS WITH FORMER PUBLIC SERVANTS - N/A .....	12
6.7 PAYMENT .....	12
6.8 INVOICING INSTRUCTIONS .....	13
6.9 CERTIFICATIONS AND ADDITIONAL INFORMATION .....	13
6.10 APPLICABLE LAWS .....	13
6.11 PRIORITY OF DOCUMENTS .....	14
6.12 DEFENCE CONTRACT N/A .....	14
6.13 SACC <i>MANUAL</i> CLAUSES .....	14
<b>ANNEX A .....</b>	<b>15</b>
STATEMENT OF WORK .....	15
<b>ANNEX B .....</b>	<b>16</b>

Solicitation No. - N° de l'invitation  
F2599-175002/A  
Client Ref. No. - N° de réf. du client  
F2599-175002

Amd. No. - N° de la modif.  
File No. - N° du dossier  
055ml.F2599-175002

Buyer ID - Id de l'acheteur  
055ml  
CCC No./N° CCC - FMS No./N° VME

---

FINANCIAL BID PRESENTATION SHEET .....	16
<b>ANNEX C .....</b>	<b>17</b>
PART 3 OF THE BID SOLICITATION .....	17
<b>ANNEX D .....</b>	<b>18</b>
<b>DELIVERABLES/CERTIFICATIONS .....</b>	<b>18</b>

## **PART 1 - GENERAL INFORMATION**

### **1.1 Security Requirements**

There is no security requirement applicable to the solicitation.

### **1.2 Statement of Requirement**

The requirement is detailed under Article 6.2 of the resulting contract clauses.

### **1.3 Comprehensive Land Claims Agreement(s) - N/A**

### **1.4 Set-aside Under the Federal Government Procurement Strategy for Aboriginal Business (PSAB) – N/A**

### **1.5 Debriefings**

Bidders may request a debriefing on the results of the bid solicitation process. Bidders should make the request to the Contracting Authority within 15 working days from receipt of the results of the bid solicitation process. The debriefing may be in writing, by telephone or in person.

### **1.6 National Security Exception – N/A**

### **1.7 Trade Agreements**

The requirement is subject to the provisions of the North American Free Trade Agreement (NAFTA) and the Agreement on Internal Trade (AIT).

### **1.8 Canadian Content – N/A**

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## PART 2 - BIDDER INSTRUCTIONS

### 2.1 Standard Instructions, Clauses and Conditions

All instructions, clauses and conditions identified in the bid solicitation by number, date and title are set out in the *Standard Acquisition Clauses and Conditions Manual* (<https://buyandsell.gc.ca/policy-and-guidelines/standard-acquisition-clauses-and-conditions-manual>) issued by Public Works and Government Services Canada.

Bidders who submit a bid agree to be bound by the instructions, clauses and conditions of the bid solicitation and accept the clauses and conditions of the resulting contract.

The 2003 (2016-04-04) Standard Instructions - Goods or Services - Competitive Requirements, are incorporated by reference into and form part of the bid solicitation.

### 2.2 Submission of Bids

Bids must be submitted only to Public Works and Government Services Canada (PWGSC) Bid Receiving Unit by the date, time and place indicated on page 1 of the bid solicitation.

Due to the nature of the bid solicitation, bids transmitted by facsimile to PWGSC will not be accepted.

#### 2.2.1 Instructions to Bidders / Contractors

Should bidders consider that the specifications or Statement of Work contained in the bid solicitation could be improved technically or technologically, bidders are invited to make suggestions, in writing, to the Contracting Authority named in the bid solicitation. Bidders must clearly outline the suggested improvement as well as the reason for the suggestion. Suggestions that do not restrict the level of competition nor favour a particular bidder will be given consideration provided they are submitted to the Contracting Authority at least 10 days before the bid closing date. Canada will have the right to accept or reject any or all suggestions.

### 2.3 Former Public Servant – N/A

### 2.4 Enquiries - Bid Solicitation

All enquiries must be submitted in writing to the Contracting Authority no later than **five (5) Working Days** before the bid closing date. Enquiries received after that time may not be answered.

Bidders should reference as accurately as possible the numbered item of the bid solicitation to which the enquiry relates. Care should be taken by Bidders to explain each question in sufficient detail in order to enable Canada to provide an accurate answer. Technical enquiries that are of a proprietary nature must be clearly marked "proprietary" at each relevant item. Items identified as "proprietary" will be treated as such except where Canada determines that the enquiry is not of a proprietary nature. Canada may edit the question(s) or may request that the Bidder do so, so that the proprietary nature of the question(s) is eliminated, and the enquiry can be answered to all Bidders. Enquiries not submitted in a form that can be distributed to all Bidders may not be answered by Canada.

Any clarifications or changes to the bid solicitation resulting from the questions and answers will be included as an amendment to the bid solicitation.

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## 2.5 Applicable Laws

Any resulting contract must be interpreted and governed, and the relations between the parties determined, by the laws in force in Ontario.

Bidders may, at their discretion, substitute the applicable laws of a Canadian province or territory of their choice without affecting the validity of their bid, by deleting the name of the Canadian province or territory specified and inserting the name of the Canadian province or territory of their choice. If no change is made, it acknowledges that the applicable laws specified are acceptable to the Bidders.

## 2.6 Optional Site Visit – Vessel

It is recommended that the Bidder or a representative of the Bidder visit the work site. Arrangements have been made for a tour of the work site onboard the vessel. The vessel will be available for site visit on **February 22, 2017, from 9h00 am to 5h00 pm, Port Colborne, ON (West Street)**. Bidders must communicate with the Contracting Authority **no later than Monday February 20, 2017** before the scheduled visit to confirm attendance and provide the name(s) of the person(s) who will attend.

Another visit is scheduled for **March 22, 2017 at the same time and address**. The same conditions applies and the Bidders must communicate with the Contracting Authority **no later than Monday March 20, 2017** to confirm their attendance.

Bidders who do not confirm attendance and provide the name(s) of the person(s) who will attend as required will not be allowed access to the site. The Contracting Authority will provide bidders, who will have confirmed their presence, with additional security directives that will need to be respected during the site visit. Bidders who do not attend or send a representative will not be given an alternative appointment but they will not be precluded from submitting a bid. Any clarifications or changes to the bid solicitation resulting from the site visit will be included as an amendment to the bid solicitation.

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## PART 3 - BID PREPARATION INSTRUCTIONS

### 3.1 Bid Preparation Instructions

Canada requests that Bidders provide their bid in separately bound sections as follows:

Section I: Technical Bid (1 hard copy and 1 soft copy on a USB Key)

Section II: Financial Bid (1 hard copy and 1 soft copy on a USB Key)

Section III: Certifications (1 hard copy and 1 soft copy on a USB Key)

If there is a discrepancy between the wording of the soft copy and the hard copy, the wording of the hard copy will have priority over the wording of the soft copy.

Prices must appear in the financial bid only. No prices must be indicated in any other section of the bid.

Canada requests that Bidders follow the format instructions described below in the preparation of their bid:

- (a) use 8.5 x 11 inch (216 mm x 279 mm) paper;
- (b) use a numbering system that corresponds to the bid solicitation.

In April 2006, Canada issued a policy directing federal departments and agencies to take the necessary steps to incorporate environmental considerations into the procurement process [Policy on Green Procurement](http://www.tpsgc-pwgsc.gc.ca/ecologisation-greening/achats-procurement/politique-policy-eng.html) (<http://www.tpsgc-pwgsc.gc.ca/ecologisation-greening/achats-procurement/politique-policy-eng.html>). To assist Canada in reaching its objectives, Bidders should:

- 1) use 8.5 x 11 inch (216 mm x 279 mm) paper containing fibre certified as originating from a sustainably-managed forest and containing minimum 30% recycled content; and
- 2) use an environmentally-preferable format including black and white printing instead of colour printing, printing double sided/duplex, using staples or clips instead of cerlox, duotangs or binders.

#### Section I: Technical Bid

In their technical bid, Bidders should explain and demonstrate how they propose to meet the requirements and how they will carry out the Work.

#### Section II: Financial Bid

Bidders must submit their financial bid in accordance with the Basis of Payment. The total amount of Applicable Taxes must be shown separately.

#### 3.1.1 Electronic Payment of Invoices – Bid

If you are willing to accept payment of invoices by Electronic Payment Instruments, complete Annex C Electronic Payment Instruments, to identify which ones are accepted.

If Annex C Electronic Payment Instruments is not completed, it will be considered as if Electronic Payment Instruments are not being accepted for payment of invoices.

Acceptance of Electronic Payment Instruments will not be considered as an evaluation criterion.

Solicitation No. - N° de l'invitation  
F2599-175002/A  
Client Ref. No. - N° de réf. du client  
F2599-175002

Amd. No. - N° de la modif.  
File No. - N° du dossier  
055ml.F2599-175002

Buyer ID - Id de l'acheteur  
055ml  
CCC No./N° CCC - FMS No./N° VME

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### **3.1.2 Exchange Rate Fluctuation**

C3011T (2013-11-06), Exchange Rate Fluctuation

### **3.1.3 SACC Manual Clauses**

#### **Section III: Certifications**

Bidders must submit the certifications and additional information required under Part 5.

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## PART 4 - EVALUATION PROCEDURES AND BASIS OF SELECTION

### 4.1 Evaluation Procedures

- (a) Bids will be assessed in accordance with the entire requirement of the bid solicitation including the technical and financial evaluation criteria.
- (b) An evaluation team composed of representatives of Canada will evaluate the bids.

#### 4.1.1 Technical Evaluation

##### 4.1.1.1 Mandatory Technical Criteria

Each bid will be reviewed to determine whether it meets the mandatory requirements of the bid solicitation. Any element of the bid solicitation identified with the words "must" or "mandatory" is a mandatory requirement. Bids that do not comply with each and every mandatory requirement will be declared non-responsive and disqualified. The mandatory requirements are as follows:

- (a) Mandatory deliverables that must be submitted with the Bidder's bid to be deemed responsive are summarized in Annex "D";

#### 4.1.2 Financial Evaluation

SACC Manual Clause [A0222T](#) (2014-06-26), Evaluation of Price.

### 4.2 Basis of Selection

#### 4.2.1 Mandatory Technical Criteria

A bid must comply with the requirements of the bid solicitation and meet all mandatory technical evaluation criteria to be declared responsive. The responsive bid with the lowest evaluated price will be recommended for award of a contract.

Bidders should note that all contract awards are subject to Canada's internal approvals process, which includes a requirement to approve funding in the amount of any proposed Contract. Notwithstanding that a Bidder may have been recommended for award of Contract, issuance of any Contract will be contingent upon internal approval in accordance with Canada's policies. If such approval is not given, no Contract will be awarded.

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## PART 5 – CERTIFICATIONS AND ADDITIONAL INFORMATION

Bidders must provide the required certifications and additional information to be awarded a contract.

The certifications provided by Bidders to Canada are subject to verification by Canada at all times. Unless specified otherwise, Canada will declare a bid non-responsive, or will declare a contractor in default if any certification made by the Bidder is found to be untrue whether made knowingly or unknowingly, during the bid evaluation period or during the contract period.

The Contracting Authority will have the right to ask for additional information to verify the Bidder's certifications. Failure to comply and to cooperate with any request or requirement imposed by the Contracting Authority will render the bid non-responsive or constitute a default under the Contract.

### 5.1 Certifications Required with the Bid

Bidders must submit the following duly completed certifications as part of their bid.

#### 5.1.1 Integrity Provisions - Declaration of Convicted Offences

In accordance with the *Ineligibility and Suspension Policy* (<http://www.tpsgc-pwgsc.gc.ca/ci-if/politique-policy-eng.html>), the Bidder must provide with its bid the required documentation, as applicable, to be given further consideration in the procurement process.

#### 5.1.2 Additional Certifications Required with the Bid

##### 5.1.2.1 Bidder's Competencies

The Bidder must demonstrate his competencies by furnishing two examples of ship equipment repair or the manufacturing of ship equipment within the past five years. The scope of work should be similar in size and complexity to those described in Annex A of this solicitation.

### 5.2 Certifications Precedent to Contract Award and Additional Information

The certifications and additional information listed below should be submitted with the bid, but may be submitted afterwards. If any of these required certifications or additional information is not completed and submitted as requested, the Contracting Authority will inform the Bidder of a time frame within which to provide the information. Failure to provide the certifications or the additional information listed below within the time frame provided will render the bid non-responsive.

#### 5.2.1 Integrity Provisions – Required Documentation

In accordance with the *Ineligibility and Suspension Policy* (<http://www.tpsgc-pwgsc.gc.ca/ci-if/politique-policy-eng.html>), the Bidder must provide the required documentation, as applicable, to be given further consideration in the procurement process.

#### 5.2.2 Federal Contractors Program for Employment Equity - Bid Certification

By submitting a bid, the Bidder certifies that the Bidder, and any of the Bidder's members if the Bidder is a Joint Venture, is not named on the Federal Contractors Program (FCP) for employment equity "FCP Limited Eligibility to Bid" list available at the bottom of the page of the [Employment and Social Development Canada \(ESDC\) - Labour's](http://www.esdc.gc.ca/en/jobs/workplace/human_rights/employment_equity/federal_contractor_program.page?&_ga=1.229006812.1158694905.1413548969) website ([http://www.esdc.gc.ca/en/jobs/workplace/human\\_rights/employment\\_equity/federal\\_contractor\\_program.page?&\\_ga=1.229006812.1158694905.1413548969](http://www.esdc.gc.ca/en/jobs/workplace/human_rights/employment_equity/federal_contractor_program.page?&_ga=1.229006812.1158694905.1413548969)).

Solicitation No. - N° de l'invitation  
F2599-175002/A  
Client Ref. No. - N° de réf. du client  
F2599-175002

Amd. No. - N° de la modif.  
File No. - N° du dossier  
055ml.F2599-175002

Buyer ID - Id de l'acheteur  
055ml  
CCC No./N° CCC - FMS No./N° VME

---

Canada will have the right to declare a bid non-responsive if the Bidder, or any member of the Bidder if the Bidder is a Joint Venture, appears on the "FCP Limited Eligibility to Bid" list at the time of contract award.

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## PART 6 - RESULTING CONTRACT CLAUSES

The following clauses and conditions apply to and form part of any contract resulting from the bid solicitation.

### 6.1 Security Requirements

6.1.1 There is no security requirement applicable to the Contract.

### 6.2 Statement of Requirement

CCG has a requirement to replace the drive components on the existing Clarke Chapman Bow Anchor Windlass, which is fitted to the CCGS Griffon.

CCG has a requirement for a new electric drive system that is compatible with the fitted Windlass and ship and that will ensure future reliability.

The Contractor must provide new equipment in accordance with the Requirement at Annex A.

### 6.3 Standard Clauses and Conditions

All clauses and conditions identified in the Contract by number, date and title are set out in the [Standard Acquisition Clauses and Conditions Manual](https://buyandsell.gc.ca/policy-and-guidelines/standard-acquisition-clauses-and-conditions-manual) (https://buyandsell.gc.ca/policy-and-guidelines/standard-acquisition-clauses-and-conditions-manual) issued by Public Works and Government Services Canada.

#### 6.3.1 General Conditions

[2010A](#) (2016-04-04), General Conditions - Goods (Medium Complexity), apply to and form part of the Contract.

### 6.4 Term of Contract

#### 6.4.1 Delivery Date

All the deliverables must be received on or before May 31, 2017.

#### 6.4.2 Delivery Points

Delivery of the requirement will be made to delivery point(s) specified below:

Attention: Chief Engineer  
CCGS Griffon  
Canadian Coast Guard  
401 King street West  
Prescott, On K0E 1T0

### 6.5 Authorities

#### 6.5.1 Contracting Authority

The Contracting Authority for the Contract is:

Name: Sidi Mohammed Belcaid  
Title: Internal Officer  
Public Works and Government Services Canada

Solicitation No. - N° de l'invitation  
F2599-175002/A  
Client Ref. No. - N° de réf. du client  
F2599-175002

Amd. No. - N° de la modif.  
File No. - N° du dossier  
055ml.F2599-175002

Buyer ID - Id de l'acheteur  
055ml  
CCC No./N° CCC - FMS No./N° VME

Acquisitions Branch  
Directorate: Refit, Logistics and Small Vessel Construction  
Address: 11 Laurier Street, Gatineau (QC) K1A 0S5

Telephone: 819-420-2292  
E-mail address: [sidi-mohammed.belcaid@tpsgc-pwgsc.gc.ca](mailto:sidi-mohammed.belcaid@tpsgc-pwgsc.gc.ca)

The Contracting Authority is responsible for the management of the Contract and any changes to the Contract must be authorized in writing by the Contracting Authority. The Contractor must not perform work in excess of or outside the scope of the Contract based on verbal or written requests or instructions from anybody other than the Contracting Authority.

### 6.5.2 Technical Authority

The Technical Authority for the Contract is:

Name: Chris Hawksworth  
Telephone: 519-383-1927  
E-mail: [chris.hawksworth@dfo-mpo.gc.ca](mailto:chris.hawksworth@dfo-mpo.gc.ca)

The Technical Authority named above is the representative of the department or agency for whom the Work is being carried out under the Contract and is responsible for all matters concerning the technical content of the Work under the Contract. Technical matters may be discussed with the Technical Authority, however the Technical Authority has no authority to authorize changes to the scope of the Work. Changes to the scope of the Work can only be made through a contract amendment issued by the Contracting Authority.

### 6.6 Proactive Disclosure of Contracts with Former Public Servants - N/A

### 6.7 Payment

#### 6.7.1 Basis of Payment

In consideration of the Contractor satisfactorily completing all of its obligations under the Contract, the Contractor will be paid a firm price as specified in Annex A for a cost of \$ \_\_\_\_\_. Customs duties are excluded and Applicable Taxes are extra.

Cost includes line item 10 of table 2 of Annex B for estimated shipping, insurance, brokerage and customs.

Canada will not pay the Contractor for any design changes, modifications or interpretations of the Work, unless they have been approved, in writing, by the Contracting Authority before their incorporation into the Work.

#### 6.7.2 Single Payment

SACC Manual clause H1000C (2008-05-12), Single Payment

#### 6.7.3 SACC Manual Clauses

SACC Manual clause C2000C (2007-11-30), Taxes - Foreign-based Contractor

SACC Manual clause C2605C (2008-05-12), Canadian Customs Duties and Sales Tax - Foreign-based Contractor

#### **6.7.4 Electronic Payment of Invoices – Contract**

The Contractor accepts to be paid using any of the following Electronic Payment Instrument(s):

- a. Visa Acquisition Card;
- b. MasterCard Acquisition Card;
- c. Direct Deposit (Domestic and International);
- d. Electronic Data Interchange (EDI);
- e. Wire Transfer (International Only);
- f. Large Value Transfer System (LVTS) (Over \$25M)

#### **6.8 Invoicing Instructions**

1. The Contractor must submit invoices in accordance with the section entitled "Invoice Submission" of the general conditions. Invoices cannot be submitted until all work identified in the invoice is completed.

Each invoice must be supported by:

- . Copy of the invoices, receipts, vouchers for all direct expenses.

2. Invoices must be distributed as follows:

- a. The original and one (1) copy must be forwarded to the address shown on page 1 of the Contract for certification and payment.
- b. One (1) copy must be forwarded to the Contracting Authority identified under the section entitled "Authorities" of the Contract.

#### **6.9 Certifications and Additional Information**

##### **6.9.1 Compliance**

Unless specified otherwise, the continuous compliance with the certifications provided by the Contractor in its bid or precedent to contract award, and the ongoing cooperation in providing additional information are conditions of the Contract and failure to comply will constitute the Contractor in default. Certifications are subject to verification by Canada during the entire period of the Contract.

##### **6.9.2 Status of equipment provided**

The Contractor must provide the Contracting Authority with evidence that all components of the Windlass are new and manufactured recently (less than 3 years).

Canada will not accept equipment refurbished, reworked or rebuilt.

#### **6.10 Applicable Laws**

The Contract must be interpreted and governed, and the relations between the parties determined, by the laws in force in \_\_\_\_\_. *(The contracting authority will insert the name of the province at Contract Award as specified by the bidder in its bid)*

Solicitation No. - N° de l'invitation  
F2599-175002/A  
Client Ref. No. - N° de réf. du client  
F2599-175002

Amd. No. - N° de la modif.  
File No. - N° du dossier  
055ml.F2599-175002

Buyer ID - Id de l'acheteur  
055ml  
CCC No./N° CCC - FMS No./N° VME

---

### 6.11 Priority of Documents

If there is a discrepancy between the wording of any documents that appear on the list, the wording of the document that first appears on the list has priority over the wording of any document that subsequently appears on the list.

- (a) the Articles of Agreement;
- (b) 2010A (2016-04-04), General Conditions - Goods (Medium Complexity), apply to and form part of the Contract.
- (c) Annex A, Statement of Work;
- (d) Annex B, Financial Bid Presentation Sheet;
- (e) Annex C, Electronic Payment Instruments;
- (f) Annex D, Deliverables/Certifications;
- (g) the Contractor's bid dated \_\_\_\_\_ (*The contracting authority will insert the date as specified by the bidder in its bid*)

### 6.12 Defence Contract N/A

### 6.13 SACC Manual Clauses

B7500C (2006-06-16), Excess Goods

B1501C (2006-06-16), Electrical Equipment

Solicitation No. - N° de l'invitation  
F2599-175002/A  
Client Ref. No. - N° de réf. du client  
F2599-175002

Amd. No. - N° de la modif.  
File No. - N° du dossier  
055ml.F2599-175002

Buyer ID - Id de l'acheteur  
055ml  
CCC No./N° CCC - FMS No./N° VME

---

## **ANNEX A**

### **STATEMENT OF WORK**

**CCGS Griffon  
Bow Anchor Windlass Drive  
Canadian Coast Guard**

## ANNEX B

### Financial Bid Presentation Sheet

**Table 1: Parts List for Anchor Windlass**

Item	Brand	Reference / Model	Description	Qty	Price
1			AC Drive Motor: TENV IP56 Marine Brake Motor, 54 KW @ 91 FLA, 440/3/60	1	\$
2			Electric motor base frame (LRS approved material)	1	\$
3			Junction box to electric motor	1	\$
4			Flexible shaft coupling to suit existing windlass shaft.	1	\$
5			Adaptable worm-gear gear box to receive and mount AC electric motor.	1	\$
6			Variable Frequency Drive cabinet and brake resistor to suit AC Motor	1	\$
7			New controls and operator console to suit new AC electric motor and VFD drive operation.	1	\$
8			VFD electric cabling to suit	50m	\$
9			Certification is required (i.e., Lloyd's Register or similar Class Approval)	1	\$
				<b>Total (taxes excluded)</b>	<b>\$</b>

**Table 2: Shipping, insurance, brokerage and customs**

Item	Description	Cost
10	Shipping, insurance, brokerage and customs	\$

Solicitation No. - N° de l'invitation  
F2599-175002/A  
Client Ref. No. - N° de réf. du client  
F2599-175002

Amd. No. - N° de la modif.  
File No. - N° du dossier  
055ml.F2599-175002

Buyer ID - Id de l'acheteur  
055ml  
CCC No./N° CCC - FMS No./N° VME

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## ANNEX C

### PART 3 OF THE BID SOLICITATION

#### ELECTRONIC PAYMENT INSTRUMENTS

*As indicated in Part 3, clause 3.1.2, the Bidder must identify which electronic payment instruments they are willing to accept for payment of invoices.*

The Bidder accepts any of the following Electronic Payment Instrument(s):

- ☐ ( ) VISA Acquisition Card;
- ☐ ( ) MasterCard Acquisition Card;
- ☐ ( ) Direct Deposit (Domestic and International);
- ☐ ( ) Electronic Data Interchange (EDI);
- ☐ ( ) Wire Transfer (International Only);
- ☐ ( ) Large Value Transfer System (LVTS) (Over \$25M)

## ANNEX D

### DELIVERABLES/CERTIFICATIONS

#### Mandatory Tender Deliverables Check List

Notwithstanding deliverable requirements specified within the bid solicitation and its associated Technical Specification (Annex A), mandatory deliverables that must be submitted with the Bidder's tender to be deemed responsive are summarized below.

The Bidder must submit a completed Annex "D" Deliverables/ Certifications.

The following are mandatory and the Bidder's submission will be evaluated against the requirements as defined herein. The Bidder must be determined to be compliant on each item to be considered responsive.

Item	Description	Completed and Attached
1	Request for Proposal document part 1 page 1 completed and signed;	
2	1 hard copy and 1 soft copy for all 3 sections, Clause 3.1;	
3	Completed Annex "B" Financial Bid Presentation Sheet"	
4	Completed Annex "D" Deliverables/Certifications;	
5	Changes to Applicable Laws (if any), as per clause 2.5;	
6	Integrity Provisions - Associated Information, Clause 5.2.1;	
7	Federal Contractors Program for Employment Equity, Clause 5.2.2;	
8	Bidder's Competencies, Clause 5.1.2.1;	
9	The tenderer must attest that he / she has read the section: Status of equipment provided, clause 6.9.2.	

CCGSGriffon  
Bow Anchor Windlass Drive Components  
Canadian Coast Guard



Revision 2 -10 February 2017

Table of Contents

1.0 Introduction- Background .....3

2.0 General Overview .....4

3.0 Technical Review and Proposed Solution .....6

    3.1 REQUIREMENT (see ANNEX A) .....8

4.0 Detailed Design Specifications.....10

    4.1 Environmental Conditions .....10

    4.2 Certifications .....10

    4.3 Electrical .....10

    4.4 Paint and Corrosion Protection.....11

    4.5 Structural Components - Welding .....11

5.0 Sizing, Testing and Documentation .....12

    5.1 Testing .....13

    5.2 Documentation .....13

ANNEX A REQUIREMENT .....14

## 1.0 Introduction- Background

CCGS Griffon has a Clarke Chapman Anchor Windlass that is now forty seven years old. The Windlass is original to the ship.

Due to the age of the equipment, maintenance and reliability has become an ongoing concern for such an important component.

OEM parts are hard to obtain if not already obsolete, presenting another hardship to the vessel Chief Engineer.

The windlass is electric motor driven and is fitted with two anchor cable lifters and two warp-ends at either end of an intermediate horizontal drive shaft

Manufacturer: Clarke Chapman

Gear # 47583

Electrical: 72 HP, 1160/2050 RPM, 220V, 262 Amps.



C.2693

### LLOYD'S REGISTER OF SHIPPING

RP

Port NEWCASTLE UPON TYNE

Date 5th December, 1968

This is to Certify that at the request of Messrs. Clarke Chapman & Co. Limited, the undersigned Surveyor did attend their Works at Gateshead, Co. Durham, on 26th November, 1968, for the purpose of examining and testing One Ford. Electric Windlass made to Messrs. Davie Shipbuilding Limited, Quebec, Canada, Order No. 654-2045-1 and intended for Ship No. 664.

The Windlass is of the makers Class 'A' type suitable for 1.5/8" dia. cable operated by a Clarke Chapman D.C. Motor Serial No. 47584 72 H.P., 1160/2050 R.P.M. 220/375 Volts, 262 amps, Shunt wound, rated 1 hr. I.L.U. 70 C. F.L.W.T., Clarke Chapman Ward Leonard A.C. Motor Serial No. 5590 88 H.P. at 1800 R.P.M., 440 volts, 3 phase, 60 cycles, 161 amps, rated 1 hr. L.C. 65°C. D.V.D.P. Clarke Chapman Ward Leonard D.C. Generator Serial No. 47586 57.6 H.P. at 1800 R.P.M., 220/375 volts, 262 amps, Compound wound, rated 1 hr. L.C. 60°C. A.V.D.P. All above equipment with Class 'B' insulation.

Slipping clutch set at 395 amps.

On completion, the windlass was examined at rest and tested under light load running conditions and the general performance was considered satisfactory.

So far as could be seen, the materials and workmanship are good.

For the purpose of identification the windlass was stamped:-

Lloyd's Test  
1135 NW  
26.11.68. R.C.B.

*R.C. Barron*  
SURVEYOR TO LLOYD'S REGISTER OF SHIPPING.  
R.C. Barron.

This Certificate is issued upon the terms of the Rules and Regulations of the Society, which provide that:-  
"The Constitution of the Society and the Rules and Regulations in respect that the Signatures of the Surveyors are properly recorded, but it is not the intention of the Society that the Surveyors are liable for any loss or damage to the property of the vessel or its cargo."

## 2.0 General Overview

CCG has a requirement to replace the drive components on the existing Clarke Chapman Bow Anchor Windlass, which is fitted to the CCGS Griffon.

All components supplied must be new and recently manufactured (less than 3 years).

CCG has a requirement for a new electric drive system that is compatible with the fitted Windlass and ship and that must ensure future reliability.

CCG has a requirement for the Vendor to supply the following material:

1. a new AC drive marine rated brake motor,
2. motor base frame (material should be approved by any Classification Society recognized by TCMS-Transport Canada Maritime Safety).
3. junction box to electric motor,
4. a flexible flanged coupling to fit between the motor and existing gearbox shafting,
5. a stepped and sealed bell housing, with access cover to coupling,
6. a new pedestal control and stand for the operator,
7. Variable Frequency Drive and operator controls (for #6) for the new arrangement and,
8. new VFR electric cabling to suit between components.

NOTE: The Windlass gearbox shall be retained. The slipping clutch shall be removed.



### 3.0 Technical Review and Proposed Solution

#### Summary of Lloyd's Register Calculations

Minimum performance calculations are performed in accordance with Lloyd's Register - Rules and Regulations for the Classification of Ships (July 2014) Part 3, Chapter 13, Section 8.

Input conditions are taken from CCGS Griffon's existing operations. Input variables are as follows:

- Diameter of Chain ( $d_c$ ) = 41.275 mm
- Grade of Chain ( $grade$ ) = 3
- Design Depth of Anchor ( $D_a$ ) = 40 m
- Anchor Weight ( $W_a$ ) = 2310 kg
- Total Length of Chain ( $L_c$ ) = 245 m
- Chain Stopper = Present

For anchoring depths below 82.5 m ( $D_a \leq 82.5$  m), the continuous duty pull ( $P$ ), in Newton's [N], over a period of 30 minutes at a speed of 9 m/min must be a minimum of,

$$P = 47.5 d_c^2$$

The resulting minimum continuous duty pull over 30 mins for the CCGS Griffon Anchor Windlass operations is,

$$P = 80922.00 \text{ N } (\cong 8249 \text{ kg})$$

The windlass must have sufficient power over a period of 2 minutes to exert a short term pull ( $P_s$ ), in Newton's [N], greater or equal to the largest value of the following 2 cases,

- i. The short term pull must be a minimum of 1.5 times greater than the continuous duty pull

$$= 1.5 P$$

$$\therefore = 121383 \text{ N } (\cong 12373 \text{ kg})$$

- ii. The short term pull must be at a minimum equal to the anchor breakout force ( $P_a$ ),

$$= 12.18 + \frac{7.0 L_c d_c^2}{100}$$

The largest of the short term pull cases is due to case i), therefore the windlass must be cable of producing at a minimum 121383 N (12373 kg) over a period of 2 minutes.

Due to a chain stopper being present and the use of grade 3 chain, the windlass brake holding force (B), in Newton's N, must be no less than,

$$B = 8.83 d_c^2 (44 - 0.08 d_c)$$

The windlass brake holding force must be no less than,

$$B = 612221 \text{ N } (\cong 8252 \text{ kg})$$

In summary, in order to be in accordance with Lloyd's Register, the windlass must meet the following minimum performance,

Lloyd's Register Performance Requirements		
Wild Cats	Pull Continuous	8249 kg @ 9 m/min (30 ft./min)
	Pull Peak @ 2 mins	12373 kg
Brake	Brake Hold	8252 kg

#### Summary of Existing CCGS Griffon Windlass Performance

The existing anchor windlass aboard the CCGS Griffon has the following performance,

Existing CCGS Windlass Performance		
Wild Cats	Pull Continuous	13 tons @ 45 ft./min (11818 kg @ 13.7 m/min)
	Pull Continuous	5 tons @ 118 ft./min (Slack @ 208 ft./min) (4545 kg @ 36 m/min (Slack @ 63 m/min))

Note, the available power supply will be 440VAC/3/60 105.5 Amps. This is currently the supply to the generator which powers the existing DC motor. Either new Hydraulic or Electric driven options will use AC motors and the ship's 440VAC supply.

The existing windlass exceeds the minimum Lloyd's Register requirements. The existing brake hold capacity is not stated however the windlass has met Lloyd's Register certification and it's assumed the existing brake capacity at a minimum meets the Lloyd's Register requirements.

Performance Specifications @ 50 hp Electric Drive		
Wild Cat	Pull	13 000 kg @ 13.7 m/min (45 ft./min)
Warping Head	Pull	5000 kg @ 36.2 m/min (118.8 ft./min)
	Slack Line	63.4 m/min (208 ft./min)

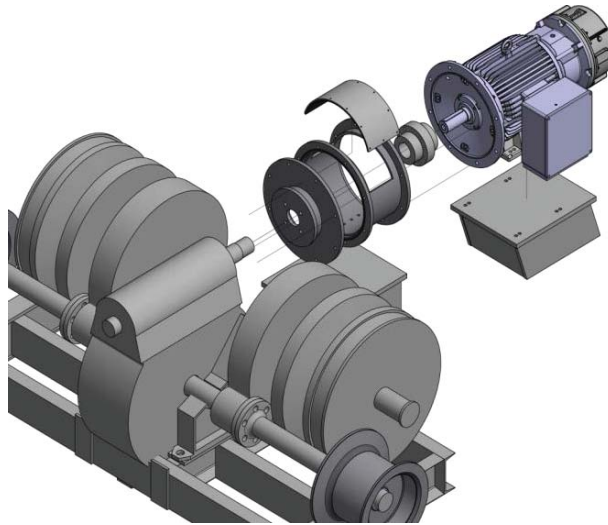
### **3.1 REQUIREMENT (see ANNEX A)**

#### **Refit Anchor Windlass–Electric (50 HP AC Motor with VFD)**

1. Replace existing windlass DC motor with a new AC marine duty electric motor. Provide an AC Drive Motor: TENV IP56 Marine Brake Motor, 54 KW @ 91 FLA, 440/3/60 c/w junction box to motor. EACH one
2. Electric motor base frame (LRS approved material or by a Classification Society recognized by TCMS). EACH one
3. Junction box to electric motor EACH one
4. Provide flexible shaft coupling to suit existing windlass shaft. EACH one
5. Provide adaptable worm-gear gear box to receive and mount AC electric motor. EACH one
6. Provide Variable Frequency Drive cabinet and brake resistor. EACH one
7. Provide new controls and operator console to accommodate new AC electric motor and VFD drive operation. EACH one
8. Provide new VFD electric cabling to suit. EACH 50 meters
9. Certification is required by a Classification Society recognized by TCMS (Transport Canada Maritime Safety).

## Recommended

### AC Motor c/w flexible shaft coupling and VFD Refit



Item 1 & 2 & 3

The AC motor must be a totally enclosed naturally ventilated, meeting IP56 standard, foot mount brake motor with D flange coupled to the existing windlass shaft. The flange must be used to support a bell housing to align the motor with the existing gear box shaft and protect the gear coupling; however, the bell housing must not be used as a structural component. The motor must be foot mounted with a new structural base (approved by a Classification Society recognized by TCMS-Transport Canada Maritime Safety) welded to the existing windlass' motor mount base.

## **4.0 Detailed Design Specifications**

### **4.1 Environmental Conditions**

The on-deck windlass components must work in all environmental and vessel operating conditions:

1. Vessel outside air temperature: –35 to 35°C, High Humidity.
2. The vessel is classified by Lloyd's. Classification number must be confirmed.
3. All motors and control panels must be a minimum of IP56/Nema4X rating.
4. Anti-condensation heaters must be using in motors and panels.

The below deck components must adhere to the following:

1. All components must meet a minimum of IP22 rating

### **4.2 Certifications**

1. All components are designed in accordance with any by a Classification Society recognized by TCMS Rules and Regulations for the Classification of Ships, July 2014. Moreover, specific attention must be given to the following:
  - Part 2 Rules for the Manufacture, Testing and Certification of Materials,
  - Part 3 Ship Structures (General), Chapters: 2 - Materials, 13 - Ship Control Systems (Sections: 7 - Equipment, 8 - Windlass Design and Testing),
  - Part 8 Rules for Ice and Cold Operations, where Winterization Class and Ice Class to be confirmed by Canadian Coast Guard.

### **4.3 Electrical**

The main electrical supply is 440VAC/3/60Hz, with panels stepped down to 220VAC/3/60 using a control transformer mounted within the control panel. Alternatively, the components using 220VAC can be sized for 110VAC/3/60 and powered direct from the vessel's auxiliary power supply.

All work is to conform to the requirements of the Transport Canada Marine Safety Directorate (TCMS) Ship Electrical Standards TP 127.

The added electrical load of the VFD driven motor along with all existing electronic components must not exceed 70% of the main genset output at any given time to avoid the need for addition line filters. The VFD at a minimum is provided with a DC choke line filter. A separate line reactor of active front end should be installed on the supply power line prior to the drive if the combined load exceeds 70% of the genset output at any given time.

Brake resistors must maintain a safe surface temperature if contacted by personnel operating in its vicinity.

All electric motors must be fitted with anti-condensation heater in windings and at bearings.

All exposed electrical components, such as joysticks, are rated IP68 at minimum.

All electrical junction boxes are rated Nema 4X or equivalent at minimum.

E-stops must be interlocked and located hard wired at the windlass control station and VFD panel.

#### **4.4 Paint and Corrosion Protection**

All components are supplied with a fully marinised paint finish.

All exposed non-painted components must be protected using grease or fabricated on non-corrosive material, such as stainless steel (316 or 17-4PH), manganese bronze, aluminum, or composite hydrolysis resistant plastic.

#### **4.5 Structural Components - Welding**

The windlass components must be constructed of materials in accordance with Lloyd's Register or any Classification Society recognized by TCMS and that equivalencies are accepted by Lloyd's Register.

The Vendor must follow an approved welding program and procedures, either established by the Canadian Welding Bureau (CWB) or another approved welding program acceptable to Lloyd's Register.

All structural steel components must be fabricated from CSA G40.21 50WT or equivalent standard. Charpy tested at -20 C to an equivalent energy level which is in accordance with any Classification Society recognized by TCMS for cold temperature applications.

## 5.0 Sizing, Testing and Documentation

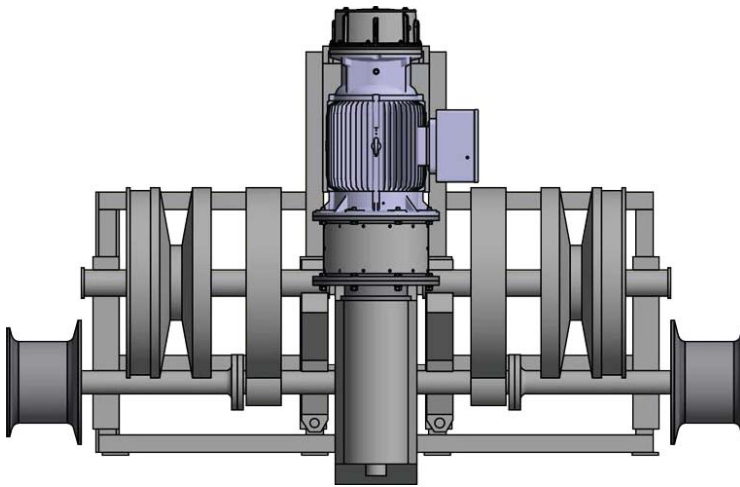
The existing windlass specification has the slip clutch currently set to 203.57 ft-lbs (~276.75 Nm). This is equivalent to 150% of the rated dynamic load of the windlass.

Therefore, the AC motor must be capable of producing 290 Nm at its nominal rating. This exceeds the minimum Lloyd's Register requirements and ensures the refit motor is capable of safely meeting the existing performance of the windlass. The motor will typically operate below its rated full load amperage but is capable of maintaining the existing windlass performance settings.

The motor must be equipped with a brake sufficient to hold 100% of the safe working load at a minimum. This exceeds the weight of the anchor and chain and must allow the windlass to hold the chain suspended while Devil's Claws are engaged. This must allow the windlass to be operated single handed.

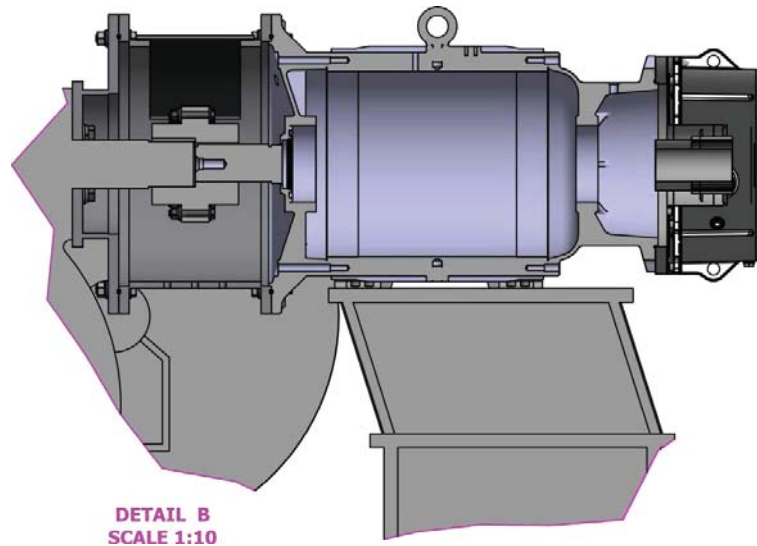
Electrical Performance	
Full Load Amperage	91 A
Power	54 kW
Full Load Power Factor	0.88
Full Load Efficiency	92.3%
Max Overload Current	148 A

The AC motor must fit within the existing envelop of the windlass.



Item 1

Mechanically the Vendor must add non-structural bell housing, flexible greased gear coupling, and mounting structures. Overall modification to the existing windlass is to be kept to a minimum.



Electrical controls on deck must be mounted in a stainless steel Nema4X control console with fabricated stand to be mounted in place of the existing control station. Existing cable runs must be reused for interconnecting wiring.

The main electrical VFD panel and brake resistor must be mounted below deck. Interconnecting wiring between the VFD panel and motor should be symmetric VFD cable with 3 power conductors and 3 ground conductors. Proper grounding is crucial to ensure electrical noise is not sent through the ships grid.

### 5.1 Testing

All component testing must be performed in accordance with any Classification Society recognized by TCMS.

### 5.2 Documentation

Two (2) hard copies and a soft copy of all manuals must be provided in pdf format. Applicable documents such as spares lists, drawing lists, AutoCAD drawings must be supplied on an USB Key. Manuals must include but are not limited to the following items:

- Lubrication Schedule,
- Recommended Spares,
- List of any special tools required for maintenance,
- General Arrangement dimensional drawings for each winch,
- General Assembly drawings of each winch,
- Written technical specifications with supporting illustrations or drawings for installation, commissioning, maintenance, repair, troubleshooting, and safety,
- FAT Test Certificate
- Any applicable test certificate or mill certification of winch sub-component

## ANNEX A REQUIREMENT

**Table 1: Parts List for Anchor Windlass**

Item	Brand	Reference / Model	Description	Qty
1			AC Drive Motor: TENV IP56 Marine Brake Motor, 54 KW @ 91 FLA, 440/3/60	1
2			Electric motor base frame (material approved by any Classification Society recognized by TCMS)	1
3			Junction box to electric motor	1
4			Flexible shaft coupling to suit existing windlass shaft.	1
5			Adaptable worm-gear gear box to receive and mount AC electric motor.	1
6			Variable Frequency Drive cabinet and brake resistor to suit AC Motor	1
7			New controls and operator console to suit new AC electric motor and VFD drive operation.	1
8			VFD electric cabling to suit	50m
9			Certification is required (Lloyd's Register or any Classification Society recognized by TCMS)	1

Note:

All Items (1 to 8) must be new and recently manufactured (less than 3 years).