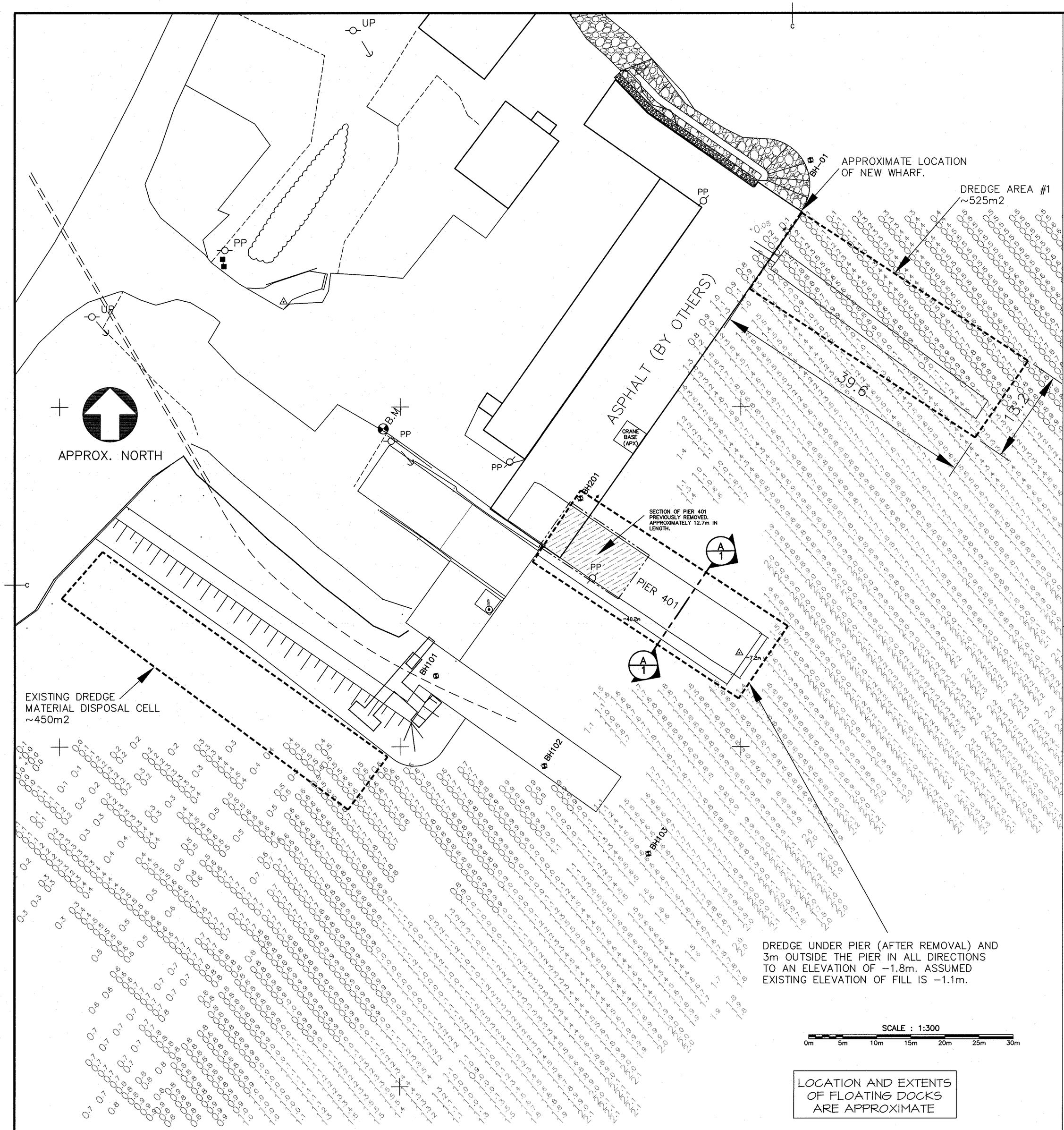
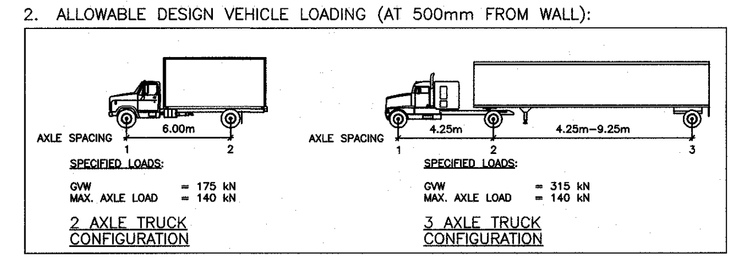


GENERAL NOTES:

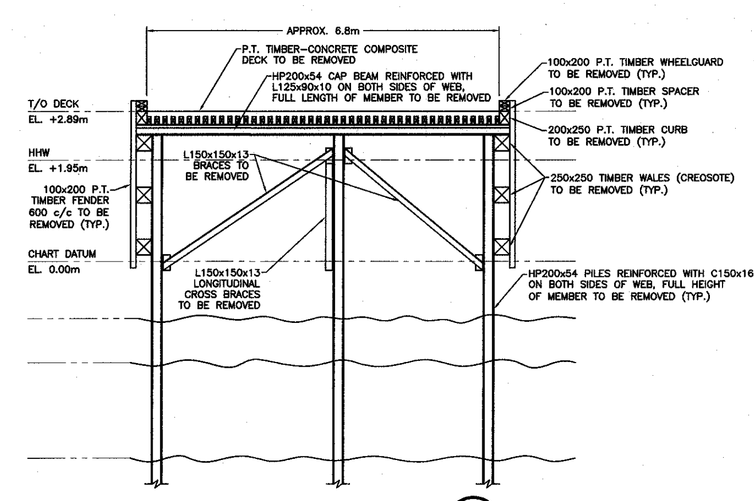
1. ALL DIMENSIONS ARE SHOWN IN MILLIMETERS AND ELEVATIONS ARE IN METERS. DO NOT SCALE FROM DRAWINGS, CALCULATED DIMENSIONS ARE TO BE USED.
2. ELEVATIONS ARE IN REFERENCE TO CHART DATUM. CHART DATUM IS, BY THE INTERNATIONAL AGREEMENT, A PLANE BELOW WHICH THE TIDE WILL SELDOM FALL. THE CANADIAN HYDROGRAPHIC SERVICE HAS ADOPTED THE PLANE OF LOWEST NORMAL TIDE (LNT) AS CHART DATUM. THE RISE, FALL AND RANGE OF TIDES VARIES DAILY, CONSEQUENTLY THE CANADIAN TIDE AND CURRENT TABLES, AS ISSUED BY THE CANADIAN HYDROGRAPHIC SERVICE, SHOULD BE CONSULTED FOR TIDAL PREDICTIONS AND OTHER TIDAL INFORMATION RELATED TO THE WORK.
3. FOR SOUNDINGS, ELEVATIONS DEPICTED WITH A (X) OR (+) ARE ABOVE 0.0M LNT DATUM AND THOSE WITHOUT ARE BELOW 0.0M LNT DATUM.
4. CONTRACTOR SHALL CONFIRM ALL DIMENSIONS AND ELEVATIONS PRIOR TO CONSTRUCTION.
5. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ADEQUATE NAVIGATION LIGHTING DURING CONSTRUCTION. CONTRACTOR IS TO COORDINATE THIS WITH DEPARTMENTAL REPRESENTATIVE AND THE HARBOUR AUTHORITY. NAVIGATION LIGHTS WILL BE DONE BY OTHERS AFTER CONSTRUCTION IS COMPLETED.
6. THERE ARE POTENTIALLY WATER INTAKES AND OTHER BURIED INFRASTRUCTURE AND UTILITIES ON SITE. DETERMINE LOCATIONS PRIOR TO DEMOLITION AND CONSTRUCTION. COORDINATE LOCATING INTAKES AND OTHER INFRASTRUCTURE WITH THE DEPARTMENTAL REPRESENTATIVE, FISHERIES CO-OP ASSOCIATION LTD., THE LOCAL HARBOUR AUTHORITY, MARITIME ELECTRIC AND OTHER UTILITY PROVIDERS.
7. REMOVE PIER 401 SHOWN IN SECTION A/1 IN ITS ENTIRETY. REMOVE ALL STEEL H-PILES, CHANNELS, LONGITUDINAL AND TRANSVERSE BRACING AND CAP BEAMS, TIMBER STRINGERS, WHALES, FENDERS, WHEELGUARDS, CURBS AND UTILITY/POWER POLES, TIMBER-CONCRETE COMPOSITE DECK AND OTHER MISCELLANEOUS MATERIALS. ALL STEEL PILES MUST BE FULLY REMOVED.
8. REINSTATE ALL DISTURBED SURFACES.
9. DISCONNECT AND RELOCATE POWER LINES AND POLES THAT INTERFERE WITH WORK. REINSTATE UPON COMPLETION OF WORK.
10. DREDGE THE INDICATED AREA TO AN ELEVATION OF −1.8m OR DEEPER.

LOADING NOTES:

1. DESIGN LOADING: THE NEW WHARF STRUCTURE SHOWN (NORTH EAST OF THE BOAT SLIP) ON THESE DRAWINGS HAS BEEN DESIGNED FOR THE FOLLOWING LIVE LOADS: UNIFORMLY DISTRIBUTED LOAD: 15.0 kN/m²
2. ALLOWABLE DESIGN VEHICLE LOADING (AT 500mm FROM WALL):



3. PIER 401 HAS BEEN BARRICADED TO ALL LOADING AND CANNOT BEAR ANY LOADING TO CARRY OUT DEMOLITION AND/OR DREDGING.



SECTION – EXISTING PIER 401 (A/1)

SCALE: 1:50

0m 1m 2m 3m 4m 5m

NOTES:

1. SECTION A/1 SHOWS A TYPICAL BENT FROM THE ORIGINAL 1963 WHARF CONSTRUCTION DRAWINGS.
2. STEEL REINFORCING WAS ADDED IN 1986 TO THE CAP BEAMS AND PILES DUE TO SEVERE SECTION LOSS.
3. IN 1995, NEW BENTS WERE ADDED ALONGSIDE EACH EXISTING BENT AND CONSISTED OF THE SAME BENT CONSTRUCTION AS THE ORIGINAL WHARF IN 1963. THESE ADDITIONAL BENTS ARE TO BE REMOVED ALSO. NO EXTRA STEEL REINFORCING WAS ADDED TO THESE BENTS.
4. ALL EXISTING IN-FIELD STRUCTURAL ELEMENTS MAY NOT BE SHOWN BUT ALL ARE TO BE REMOVED.
5. THERE IS SEVERE CORROSION OF THE EXISTING STEEL H-PILES AND ALL OTHER STEEL MEMBERS. THIS PILE CORROSION MAY COMPLICATE THE REMOVAL OF PILES. ALL PILES ARE TO BE REMOVED FULL DEPTH.
6. THIS SECTION OF WHARF HAS BEEN BARRICADED TO ALL LOADING AND CANNOT BEAR ANY LOADING TO CARRY OUT DEMOLITION AND/OR CONSTRUCTION.
7. REMOVE ALL SEDIMENT, DEBRIS, SANDSTONE, FILL AND/OR STRUCTURAL MATERIAL (OTHER THAN PILES) TO AN ELEVATION OF −1.8m. DISPOSE OF IN A PROVINCIALLY APPROVED MANNER.

REMOVAL NOTES:

1. REMOVE PIER 401 AS NOTED AND DISPOSE OF ALL MATERIALS. DREDGE MATERIAL UNDER PIER 401 TO AN ELEVATION OF −1.8m. PLACE DREDGED MATERIAL IN THE EXISTING ONSITE MATERIAL DISPOSAL CELL.
2. REMOVE ALL STEEL PILES FULL DEPTH.
3. DO NOT CAUSE DAMAGE TO ADJACENT STRUCTURES. IF DAMAGE OCCURS, REPAIR TO CONDITION EQUAL TO OR BETTER THAN PRIOR TO CONSTRUCTION.
4. AN APPROXIMATE 12.7m LONG SECTION OF THE PIER (WHERE IT ABUTTED TO THE ADJACENT WHARF) WAS REMOVED PREVIOUSLY.

Public Works and Government Services Canada / Travaux Publics et Services gouvernementaux Canada

SURVEY NOTES

SURVEY PARTY CHIEF: GERALD SPICER
SURVEY DATE: SEPTEMBER 5 2008
SURVEY METHOD: DIFFERENTIAL GPS (RTK/RTN/CDGPS)

[illegible]

DATUM: [illegible]
[illegible]

SYSTEM PROFESSIONAL: GERALD SPICER
DATE OF PROFESSIONAL: SEPTEMBER/OCTOBER-2008

[illegible]

[illegible]	[illegible]	[illegible]	[illegible]	[illegible]	[illegible]	DESCRIPTION
[illegible]	[illegible]	[illegible]	[illegible]	[illegible]	[illegible]	PIER CONTROL
[illegible]	[illegible]	[illegible]	[illegible]	[illegible]	[illegible]	PWGSC Control CHAIN PROJECTED AT BEACH PT
[illegible]	[illegible]	[illegible]	[illegible]	[illegible]	[illegible]	PWGSC Control OFFSET IN DISTO AT MARSHAL PT

DRAWING REVIEWED BY:

SIGNATURE DATE

NOTE: MARSHAL PT WHARF TIE IS ESTIMATED FROM PWGSC SURVEY 2007,2008

revisions		date
0		

project / projet

PIER 401 REMOVAL
MACHON'S POINT WHARF
MURRAY HARBOUR
KINGS COUNTY, PEI

drawing / dessin

EXISTING CONDITIONS, REMOVALS, AND SITE PLAN

designed / conçu
date

drawn / dessiné: R.A.B.
date: MARCH 2017

approved / approuvé: Jeff Keefe
date: March 13, 2017

Tender / Soumission
Jeff Keefe March 13, 2017
PWGSC Project Manager / Administrateur de projets TPSGC

project number / no. du projet: R.077232.001

drawing no. / no. du dessin: 1 OF 1

PWGSC A1 (2004)

E-DRM/GDD-E: 535601