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**SOLICITATION AMENDMENT
MODIFICATION DE L'INVITATION**

The referenced document is hereby revised; unless otherwise indicated, all other terms and conditions of the Solicitation remain the same.

Ce document est par la présente révisé; sauf indication contraire, les modalités de l'invitation demeurent les mêmes.

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Title - Sujet Agrandissement quai des traversiers	
Solicitation No. - N° de l'invitation EE519-220842/A	Amendment No. - N° modif. 005
Client Reference No. - N° de référence du client R.115132.100	Date 2021-10-26
GETS Reference No. - N° de référence de SEAG PW-\$QCM-032-18217	
File No. - N° de dossier QCM-1-44065 (032)	CCC No./N° CCC - FMS No./N° VME
Solicitation Closes - L'invitation prend fin at - à 02:00 PM Eastern Daylight Saving Time EDT on - le 2021-11-02 Heure Avancée de l'Est HAE	
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Signature	Date

AMENDMENT-005

Wharf Extension, Reinforcement, and New fenders Cap-aux-Meules, Québec

Included in the present amendment:

1. Question and answers 14 to 34
-

QUESTIONS AND ANSWERS 14 to 34 :

Question 14 : In Specifications, Section 01 11 01, 1.14, point 2, it is mentioned that: "Access to the existing wharf structure must be maintained throughout construction. Ferry vessels will continue to use the wharf structure for berthing and mooring. The existing transfer bridge will not be used throughout construction. Access for crew and emergency vehicles to vessel and wharf must be maintained at all times."

- As part of the work, we have to remove all the fenders and bollards. How do you plan to moor the ferry under these conditions?
- What is the transfer bridge?
- What space is required on the dock for crew and emergency vehicles?

Answer 14 : The temporary fenders are to be supplied by the contractor.
It is confirmed the transfer bridge is the ramp.
The required space must only allow access for crew and emergency vehicles and access to moor the vessel (use of the existing mooring bits kept in place). No public access.

Question 15 : In specification section 01 35 44, it is written that it is forbidden to do noisy works (pile driving by vibratory or hammer driving) between April 1st and May 31st. The project having to be completed by June 1st 2022, it is unavoidable to have temporary structure piles to remove during this period and probably also some driving of permanent piles.

Answer 15 : Works schedule and means to adapt according to this constraint.

Question 16 : Could you send the mooring and fender systems supplier shop drawings?

Answer 16 : These shop drawings are not presently completed.

Question 17 : Can changes be made to the specifications in order to allow the only plant in the region to produce concrete?

Answer 17 : Use of a non-automated plant is considered acceptable as long as the concrete meets the final hard state performance criteria in the specification for strength, entrained air content, and shrinkage compensation (where required). Please note that only non-reactive aggregates will be accepted. L-bar laboratory tests will be required.

Question 18 : In specification section 01 29 00 item 1.3.2, it is asked to remove sediments inside the pile up to the bottom of the pile, can you define the bottom of the pile (approximative seabed, pile toe, etc.)?

Answer 18 : It is the pile toe.

Question 19 : In specification section 01 35 29.06 item 1.13.1, it is defined that “ The worksite is occupied by employees and/or the public during the following times: once a day for two hours, although these persons will not have access to the Contractor Worksite. The Contractor shall leave a safe access to employees and / or the public during the boarding/unboarding operations. » Can you specify the access that need to be kept on the existing wharf?

Answer 19 : The required space must only allow access for crew and emergency vehicles and access to moor the vessel (use of the existing mooring bits kept in place). No public access.

Question 20 : In specification section 01 11 01 item 1.14.2, it is specified that “ Access to the existing wharf structure must be maintained throughout construction. Ferry vessels will continue to use the wharf structure for berthing and mooring. The existing transfer bridge will not be used throughout construction. Access for crew and emergency vehicles to vessel and wharf must be maintained at all times. » Can you specify the access that need to be kept on the existing wharf?

Answer 20 : The required space must only allow access for crew and emergency vehicles and access to moor the vessel (use of the existing mooring bits kept in place). No public access.

Question 21 : In specification section 01 11 01 item 1.14.2, it is specified that “ [...] Ferry vessels will continue to use the wharf structure for berthing and mooring. [...] » Can you specify the removal and installation sequence of the new fenders and bollards?

Answer 21 : Sequence at the contractor's discretion taking into account that temporary fenders will have to be supplied and installed by the contractor for the period between the removal and the installation of new fenders. Mooring bits are left in place to allow mooring without any other measures.

Question 22 : In specification section 03 30 00, it is specified that “vibrant rule or a self-propelled finisher should be used for the completion of structural slabs », given the circular shape of the dolphin and the complexity of the works, is it required to use this type of equipment?

Answer 22 : Read « vibratory concrete screed » instead of « vibrant rule ». The vibratory concrete screed is required. The self-propelled finisher can be replaced by a mechanical troweling finish.

Question 23 : In specification section 01 11 01 item 1.7.2, it is specified that “ Contractor responsibility for purchase, handling, and installation for pre-ordered products is same as for other Contractor-furnished products. » Can you specify the location where to pick up the pre-ordered equipment?

Answer 23 : The pre-ordered equipment will be delivered on the ferry site.

Question 24 : In specification section 02 41 99 item 3.2, it is mentioned “ The Departmental Representative shall designate the location from which the contractor must obtain drinking water. « Is it possible to have the location of this source, the flow rate, the output size and is it possible to use this source during the whole duration of the works?

Answer 24 : Ferry water intake is on the wharf, use to coordinate with CTMA, ferry operator; see attached picture.

Question 25 : Is it possible to move the container and the stairs located between axis 19 and 22 in drawing C04, if so what is the its weight?

Answer 25 : It is possible. The procedure will have to be submitted to the Departmental Representative. Consider the 20 feet container is empty (2230kg). See attached pictures.

Question 26 : In specification section 01 29 00, for items 1.2.4, 2.1.2 and 2.1.3, these elements include the clogging of the anchor heads. Is it possible to have some precisions on the required clogging product?

Answer 26 : It will have to be a single component anchor sealant, elastomeric polyurethane based, of medium module and of high performance.

Question 27 : Detail type 1 on drawing C33 for the fender system does not show it applicable to axis 24, is it an omission or is there another detail that applies?

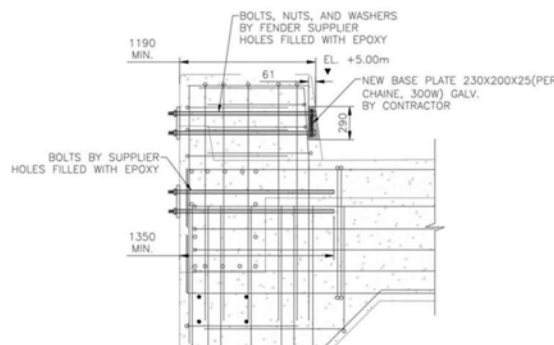
Answer 27 : This detail 1 is applicable to axis 24.

Question 28 : Are the fenders to be dismantled handed over to the client? Would it be possible to have the drawings for the H03 and H04 ladders to relocate in drawing C10?

Answer 28 : The fenders are to be disposed of.
Such ladder drawings are not in our possession. Consider at this stage a similarity with details shown on drawing C23, but with a vertical pile.

Question 29 : The answer to question 12 contradicts the information on the drawings, which information takes precedence?

Answer 29 : For the underside anchors shown on the drawing, the plates will be provided by the supplier as on the drawings. For the upper chain anchors, given they require drilling through existing concrete, we recommend, at this stage, to provide for each fender either a 1020x235x30mm back plate or two 230x200x25mm to install in a recess to execute into the existing concrete.



Question 30 : There presently are ducts on the dyke-side of the wharf that will be in conflict with the axis 7 extension cap, these are not shown on the drawings. Will these be removed before the start of the works, if not what do we need to plan?

Answer 30 : Plan to locally dismantle the ducts on the surface where the extension cap will be placed and to reinstall the ducts once the extension cap construction is completed. The connection could be

done by having the new ducts go under the underside of the extension cap or according to departmental representative directions.

Question 31 : Given that the wharf stays functional for the berthing of the CTMA, is there a minimum number of fenders that need to stay functional during the works: for instance, do 7 of the 10 fenders have to be functional during the CTMA berthing? With the demolition and reconstruction delay of the fenders including the concrete cure duration it is impossible to change a fender between two ferries.

Answer 31 : Account for temporary fenders in order to have 10 fenders anytime.

Question 32 : Is it possible to have the rebar drawings of the existing wharf in order to be able to check its bearing capacity?

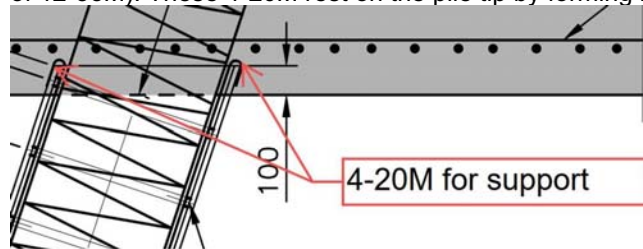
Answer 32 : See attached DWG files.

Question 33 : Following the answer to question #5 of addenda #3 about the weld inspections: can the weld inspections be done after completion of welding works or is an inspector necessary before, during and after the welding?

Answer 33 : The inspector has to plan the necessary verifications before welding, the necessary verifications during welding and the control after welding.

Question 34 : On drawing C21, Typical Detail – Pile Connection, there is a callout for 4-20M, however in section A next to that it shows 12-35M. Should those callouts match? Or is there a transition from 4-20M to 12-35M that I am not seeing a detail for?

Answer 34 : The 4-20M are construction supports forming a basket to hold the rebar cage up (itself constituted of 12-35M). These 4-20M rest on the pile tip by forming hooks:



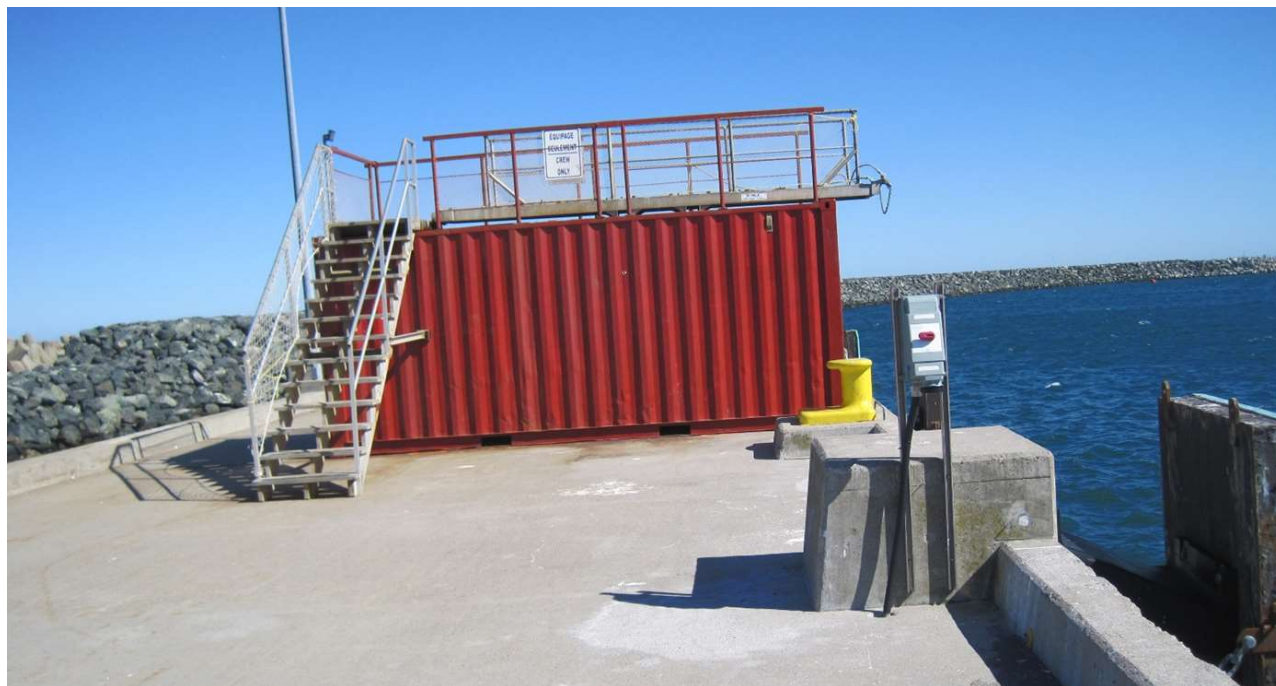
*** ALL OTHER CLAUSES AND CONDITIONS REMAIN UNCHANGED ***

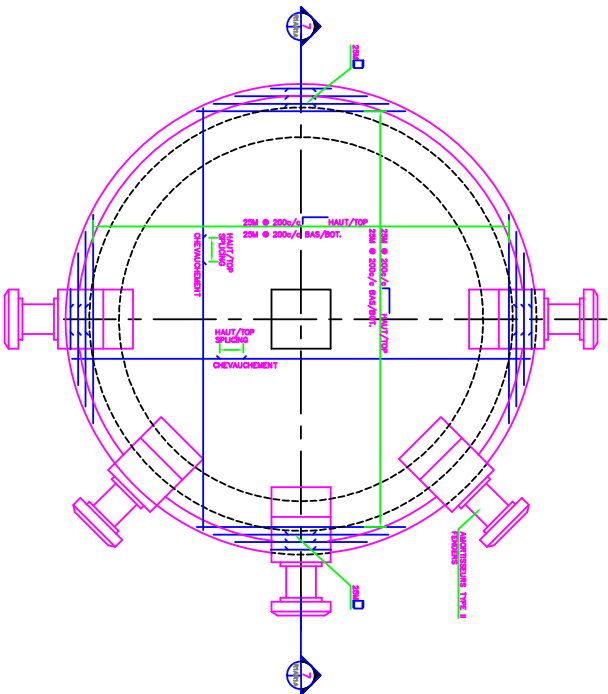
Pictures

#24



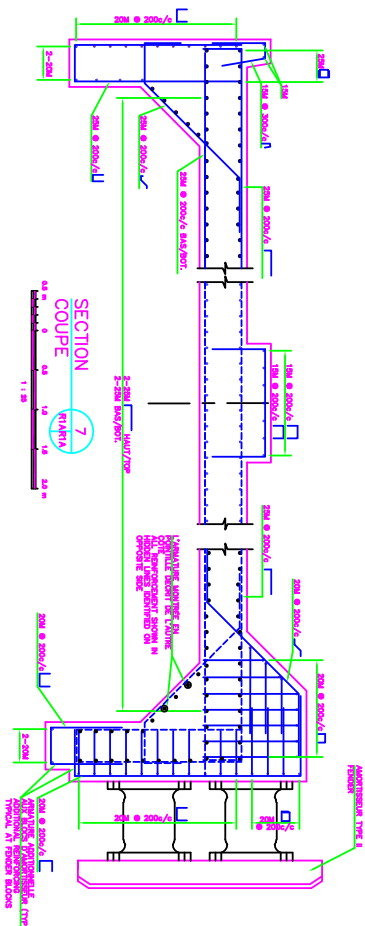
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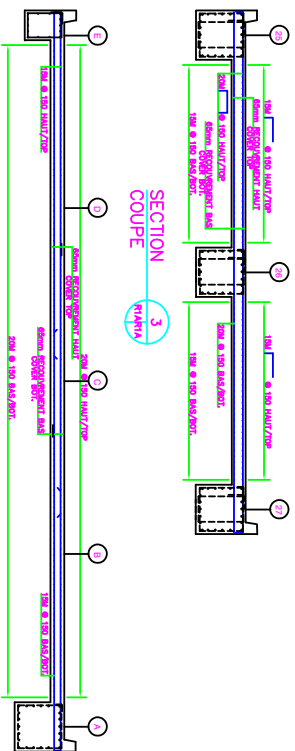


PLAN - TABLIER DE LA CELLULE
PLAN - CELL DECK

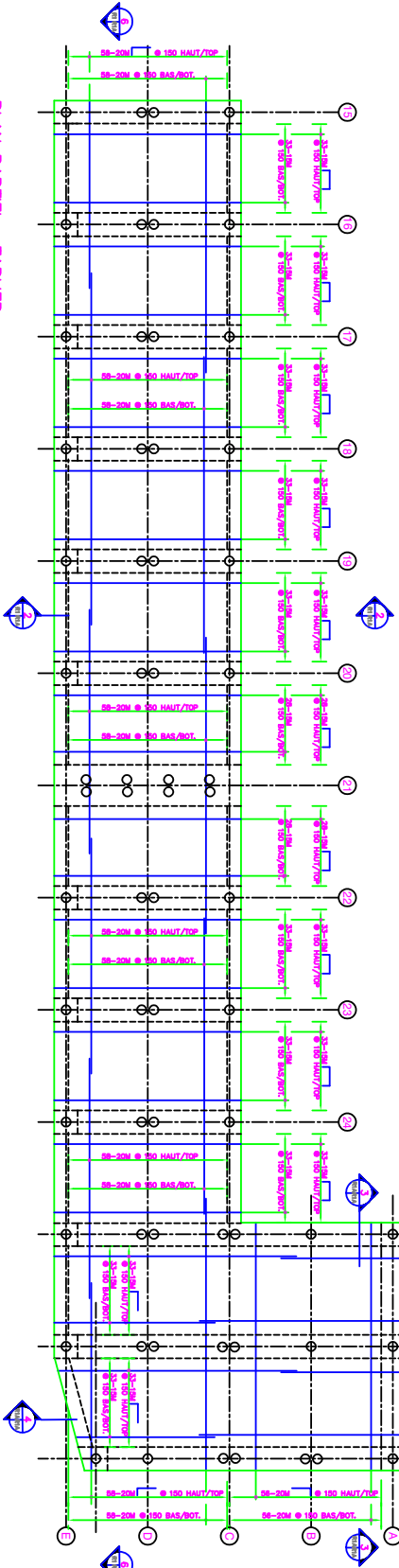
PLOT SCALE SCALE



SECTION 3
COUPE 3



SECTION 4
COUPE 4



PLAN PARTIEL - TABLIER
PART PLAN - DECK

2.00m 1.10m 2.00m

1.00m 1.10m 1.20m

1.00m 1.10m 1.20m

TEL. QUI CONSTRUCT
AS BOUTE
1998-10-20

1.00m 1.10m 1.20m

ARMATURE DU TABLIER ET
D'ALBE CIRCULANT
DECK REINFORCING
AND CIRCULAR DOLPHIN

704861
R1A QU9761M

