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**REVISION 003 TO A  
 INVITATION TO TENDER**

The referenced document is hereby revised; unless otherwise indicated, all other terms and conditions remain the same.

**Issuing Office:**  
 Parks Canada Agency  
 National Contracting Services  
 Calgary, AB

<b>Title:</b> Grice Bay & McLean Point Remediation – Pacific Rim National Park Reserve	
<b>Solicitation No.:</b> 5P468-21-0110/A	<b>Date:</b> January 20, 2022
<b>Amendment No.:</b> 003	
<b>Client Reference No.:</b> 1568	
<b>GETS Reference No.:</b> PW-21-00980058	

<b>Solicitation Closes:</b> At: 2:00 PM On: January 25, 2022	<b>Time Zone:</b> MST
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<b>F.O.B.:</b> Plant: <input type="checkbox"/> Destination: <input checked="" type="checkbox"/> Other: <input type="checkbox"/>	
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<b>Destination of Goods, Services, and Construction:</b> See Herein	

**TO BE COMPLETED BY THE BIDDER**

<b>Vendor/ Firm Name:</b>	
<b>Address:</b>	
<b>Telephone No.:</b>	<b>Fax No.:</b>
<b>Name of person authorized to sign on behalf of the Vendor/ Firm (type or print):</b>	
<b>Signature:</b>	<b>Date:</b>

## Amendment 003

This amendment is raised to distribute questions and answers and make changes to the tender package.

### A. Question and Answers

**Q3:** Will Parks Canada provide TCLP results for soil samples containing elevated PAHs concentrations?

**A3:** Summaries of available soil analytical results are found in Annex B, Environmental Information. PCA is not in a position to confirm any information outside of the findings contained within Annex B. It is the contractor's responsibility to review and bid based on the provided information.

**Q4:** Confirm that no hazardous waste is present within soil excavations and drums.

**A4:** Summaries of available soil analytical results are found in Annex B, Environmental Information. PCA is not in a position to confirm any information outside of the findings contained within Annex B. It is the contractor's responsibility to review and bid based on the provided information.

**Q5:** Are soil sample results available for soils found in drums? Should we assume soils are waste quality characterization (i.e. CSR IL+ for hydrocarbons)?

**A5:** No soil analytical results are available for soils found in drums at this time. PCA will be sampling the soil contained within the drums and will provide the analytical results to the successful proponent post-award. For the purposes of tender submission, the proponent is to assume that soil contained within the drums is waste quality.

**Q6:** Are chloride soil sample results available for the excavation areas? Should we assume chloride soil concentrations less than CSR RL standards in excavation areas?

**A6:** No soil sample analytical results outside those summarized in the project specifications and Environmental Information in Annex B are available for the excavation areas. Chloride is not a contaminant of concern for PCA, and as such no direction on assumptions will be provided.

**Q7:** Is stockpiling and ex-situ sampling of parking lot and boat ramp excavated materials required prior to use as backfill in excavations?

**A7:** Stockpiling and ex-situ sampling of parking lot excavated material is required prior to use as backfill in excavations. If material is encountered that exhibits no field screening indications of impacts, the Departmental Representative may exercise their discretion and clear the material for direct loading without stockpiling but the contractor is to assume stockpiling and ex-situ sampling for tender preparation purposes.

No stockpiling or ex-situ sampling of boat ramp excavated material is expected prior to re-use, although if field screening indicates impacts may be present, the material may require stockpiling and ex-situ sampling in which case discussion with PCA could occur.

**Q8:** Are any federal permits or approvals in place for work below the high-water mark and within the F&OC least risk timing window up to February 15<sup>th</sup>? Permits/approvals may include a Fisheries & Oceans Canada Request for Review, Species at Risk Act Notification, Impact Assessment Act Registry Notification, and Transport Canada Navigation Protection Program notification.

**A8:** A Fisheries & Oceans Canada Request for Review has been completed and approved for the anticipated scope for work below the high-water mark for the replacement of the boat launch.

**Q9:** Has public consultation and/or first nations consultations been completed for this project? Is the contractor responsible if consultation is required as part of federal permits/approvals?

**A9:** Public consultation and First Nations consultations have been undertaken for this project. PCA will liaise and consult with the public and First Nations throughout the project duration.

**Q10:** Provide a drawing of the section of the boat ramp to be removed and replaced.

**A10:** No drawing of the existing section of the boat ramp to be removed and replaced is available. Determination of compromised section of the boat launch will be at direction of the Departmental Representative. For tender estimation purposes 02 62 00.01 - BOAT RAMP REMOVAL, EXCAVATION, BACKFILLING, AND INSTALLATION, Section 1.1.1 specifies Boat Ramp Concrete Cutting and Removal for straight cut of concrete for up to 5 m in length [of boat launch length]. Note that the slabs to be installed will extend down-slope beyond the current extent of the existing ramp. Shop drawings for the slabs are found in Annex E, and photos of the full length of the boat launch including the removal section have been included in this Amendment.







**Q11:** Provide the location of the access barrier on the access road? How wide should the access barrier be?

**A11:** The barrier is to be located at the direction of the Departmental Representative, but will be approximately in the same location as the current chain barrier on the existing access road visible in the photos found in Annex A, Site Photographs. The width of the access barrier should be sufficient to prevent pedestrian and vehicular access to the McLean Rd Access Road, and is to be no less wide than the existing barrier.

**Q12:** Which five monitoring wells need to be decommissioned?

**A12:** Selection of monitoring wells for decommissioning will be determined by final excavation extents and requirements for future monitoring based on analytical results from confirmatory sampling and will be at the direction of the Departmental Representative, but the following 5 wells are expected to be decommissioned:

WP-2

WP-3

BH16-19

BH16-6

BH17-30

**Q13:** Should the entire stockpile of woody debris at Radar Hill be moved to the access road for decommissioning? The woody debris stockpile is very large and will be difficult to manage on-site given space limitations and the requirement for public use of the boat launch.

**A13:** See answer to Question #1, Amendment #2. Note that volume of Woody Debris at Radar Hill includes any aggregate materials remaining in the stockpile adjacent to the Woody Debris stockpile that are not used for final grading if Option #2 is selected by the contractor. Remaining stockpiled aggregate from Radar Hill is to be used as backfill at Grice Bay parking lot if suitable (determination of suitability by Departmental Representative) and backfill at McLean Pt if not suitable for Grice Bay parking lot.

**Q14:** Do you have an alternate location to stockpile woody debris prior to access road placement?

**A14:** See answer to Question #1, Amendment #2. Note that volume of Woody Debris at Radar Hill (01 11 00 SUMMARY OF WORK, Section 1.4.1.14.2) is to include any aggregate materials remaining in the stockpile adjacent to the Woody Debris stockpile that are not used for final grading if Option #2 is selected by the contractor. Remaining stockpiled aggregate from Radar Hill is to be used as backfill at Grice Bay parking lot if suitable (determination of suitability by Departmental Representative) and backfill at McLean Pt if not suitable for Grice Bay parking lot.

**Q15:** Confirm that the work area at McLean point will be cleared of vegetation prior to start of project.

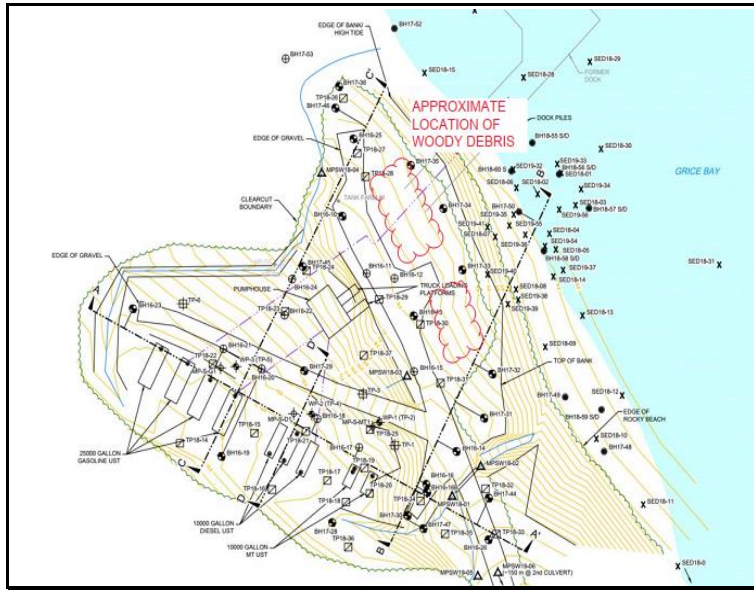
**A15:** The work area at McLean point will be cleared of standing vegetation prior to start of project. See Q&A 17 for further information.

**Q16:** The CEMP states that "excavation of soil and sediment will no occur during periods of inclement weather including rainfall". Please provide additional clarification considering that Tofino/Ucluelet average precipitation is approx. 250 mm per month over Feb-June each year.

**A16:** The intent of the CEMP is to state "...inclement weather including excessive rainfall". It is the responsibility of the contractor to respect all applicable regulations and guidance, including (but not limited to) federal and provincial regulations, sediment and erosion control measures and requirements found in the project specifications, the CEMP and BIA Mitigation Measures. Additional information can be found in 01 35 43 ENVIRONMENTAL PROCEDURES, with information on precipitation specifically in Section 1.4.

**Q17:** Can you please provide an outline of the area(s) at McLean Point that will be made free of bushes and successive growth prior to the start of this contract?

**A17:** The areas to be cleared by others at McLean Point will roughly coincide with the Tree Line perimeter indicated in the Project Figures. It should be noted that a portion of the cleared area within the Tree Line east of the access road contains Woody Debris from previous clearing efforts. This material will not be removed by others prior to the start of the contract. See markup of 123221826\_003\_Site-McLean Dwg below for clarification.



**Q18:** Can you please provide an estimated volume of contents within the USTs that are said to contain *varying amounts* as per item 1.1.1 in section 02 61 00.03?

**A18:** The current volume of the UST contents is unknown and the contractor is urged to review the relevant sections of the Project Specifications, Project Drawings, and Environmental Information in Annex B for information on the tanks, their contents, and possible upset volumes for tender estimation purposes.

**Q19:** Please clarify the type of milestones required in the project schedule – should the milestones required in the Gantt chart for work activities outlined under 1.6.3 in the Specifications be shown as completion date milestones, start date milestones, or both start and completion milestones?  
i.e.:

1.6.3. Project Schedule must include sequencing, and at a minimum the below milestones  
1.6.3.5 Excavation at each of the site locations including McLean Point and Grice Bay Remedial Excavations, including underground storage tank and fuel line infrastructure removal; Management, haulage, and disposal of contaminated material

**A19:** Gant Chart requirements of 01 32 16.07 CONSTRUCTION PROGRESS Sections 1.4 and 1.6 apply, e.g.:

1.4.5.2 Work Sequencing schedule must show on a Gantt chart, start, end and dependencies of each major task, and indicate Work to be performed in sequence and in parallel.

1.4.5.3 Major tasks include all items identified on Unit Price Table

1.6.3. Project Schedule must include sequencing, and at a minimum the below milestones:

1.6.3.1 Pre-mobilization;

1.6.3.2 Mobilization;

1.6.3.3 Site Preparation;

1.6.3.4 Contaminated Water Treatment Provision and Setup;

1.6.3.5 Excavation at each of the site locations including McLean Point and Grice Bay Remedial Excavations, including underground storage tank and fuel line infrastructure removal; Management, haulage, and disposal of contaminated material;

1.6.3.6 Contaminated water management and treatment;

1.6.3.7 Movement of re-use material from Grice Bay parking lot including peat, soil and granular material;

1.6.3.8 Boat launch reconstruction;

1.6.3.9 Parking lot reconstruction including, subgrade construction, culvert replacement, grading, asphalt paving, and painting;

1.6.3.10 Movement of woody debris from the Radar Hill parking lot;

1.6.3.11 Placement of woody debris and site rehabilitation;

- 1.6.3.12 Demobilization and final submittals; and
- 1.6.3.13 Final completion of the work.

**Q20:** Is there a specification for the type of marking paint to be used for parking lot line painting? Would this be regular traffic paint (not thermoplastic or MMA)?

**A20:** See 32 17 23 PAVEMENT MARKING, Section 2.1.1. for paint specification.

**Q21:** We would like to ask where the 3 pre-cast concrete slabs for the boat ramp reconstruction are currently located at.

**A21:** See 02 62 00.01 BOAT RAMP REMOVAL, EXCAVATION, BACKFILLING, AND INSTALLATION Section 3.8.1

**Q22:** Please clarify the role requirement for the Environmental Technologist position listed in 3.1.1 Management Team in the ITT document.

**A22:** The Environmental Technologist position listed in 3.1.1 Management Team in the ITT document is an opportunity for the contractor to present staff on their team that will assist in meeting the environmental objectives of the project, including, but not limited to, understanding remediation processes including water treatment and compliance sampling, confirmatory soil sampling, environmental compliance, and environmental monitoring. If this role is to be assumed by a team member other than a dedicated Environmental Technologist, the contractor is to identify that individual and clearly state how they are qualified and will be positioned to identify and comply with all contract environmental procedures, including all BIA mitigation measures, all regulatory approval terms and conditions and all applicable environmental legislation at all times throughout the duration of the contract. See response to Q&A 23 for more information.

**Q23:** Is the contractor responsible for environmental monitoring during the project or will Parks Canada supply an environmental monitor?

**A23:** The contractor is responsible for fully identifying and complying with all contract environmental procedures, including all BIA mitigation measures, all regulatory approval terms and conditions and all applicable environmental legislation at all times throughout the duration of the contract. As such, the contractor is to identify means, methods, and sequences to ensure compliance, including, but not limited to, monitoring. PCA will be monitoring environmental compliance to ensure that the contractor meets their responsibilities and obligations, but the presence of the PCA Environmental Monitor will not absolve the contractor from preventing, monitoring, and controlling all project activities and potentially deleterious impacts and ensuring compliance with: applicable permits, certificates, approvals, or any other form of authorizations; other federal, provincial, or municipal requirements; and in accordance with the Contract.

**Q24:** What are the requirements for public use of the boat launch during the project? How long can the boat launch be closed for parking lot and boat launch work? Are parking spaces expected to be maintained in the Grice Bay parking lot?

**A24:** As stated in 01 11 00 SUMMARY OF WORK, Section 1.7.2.2 there are Site users including roadway and boat launch users such as Tla-o-quiath First Nation and Creative Salmon that PCA has agreements for throughfare with, and the general public. PCA realizes that due to the nature of work required at the boat launch and parking lot, access for these stakeholders will be affected; however, it is a priority for PCA to work with the contractor to reduce impacts to these stakeholders as much as possible.

While PCA will allow longer term closures to the general visitation public to reduce visitation and traffic on the access roadway, there are residents that rely on the parking lot and boat launch to access their homes on a daily basis.

As such, and per 01 11 00 SUMMARY OF WORK, Section 1.4.1.1 closures must be coordinated with PCA and construction work will have to allow at a minimum for partial (morning and evening at coordinated times) pedestrian access to the boat launch during construction works that do not involve the

boat launch replacement. If required, temporary road closures of Grice Bay Road from Highway 4 are to be coordinated with PCA during hauling (e.g. long loads).

PCA proposes that access to at least the southern portion of the Grice Bay cobbled beach to the water be maintained for stakeholders, preferably at all times, and at the very least to escorted pedestrians at coordinated timing windows in the morning and evening. This will permit residents to access the shoreline for embarkation while work on the boat launch proper is being conducted, and perhaps throughout the entirety of the parking lot construction if fencing can be established in such a way to allow passage around the area under construction. While we will ask that every effort be made by the contractor to keep as much of the parking lot open for as long as possible, PCA recognizes that portions will not be usable for much of the parking lot construction periods; as such, PCA requires that the proponent allow for up to 5 temporary parking spaces along the roadway shoulder or in a portion of the parking lot not under active construction (when available) for residents that require access to the beach or boat launch to access their residences. The contractor will be responsible for signage at the entrance to the Grice Bay access road during periods when (in consultation with PCA) it is determined it is best to close general public access to non-authorized users to the site (residents and Creative Salmon employees would qualify as authorized users to the site).

In addition to accommodating residents throughout the construction period, PCA requires that the contractor make all reasonable efforts to accommodate regular access for Creative Salmon employees through the parking lot and the boat launch when possible. Due to the likelihood of limited parking, Creative Salmon may employ shuttle busses originating outside of the project area at pre-determined times in the morning and evening to allow for their employees to embark and disembark boats that will ferry them to and from Creative Salmon operations in Grice Bay.

## **B. Tender Package Revisions**

**IN:** 01 11 00 SUMMARY OF WORK 1.4.1.14.2.

**DELETE:**

Distribution of 150 m<sup>3</sup> of woody debris stockpiled at McLean Point and 2,500 m<sup>3</sup> of woody debris currently located at Radar Hill is to be transported and placed at McLean Point and along the McLean Point Access Road as part of decommissioning and site restoration.

**REPLACE WITH:**

Distribution of 150 m<sup>3</sup> of woody debris stockpiled at McLean Point and 2,500 m<sup>3</sup> of woody debris and stockpiled aggregate currently located at Radar Hill is to be transported and placed at Grice Bay, McLean Point, and along the McLean Point Access Road as part of decommissioning, backfill, and site restoration. Remaining stockpiled aggregate from Radar Hill is to be used as backfill at Grice Bay parking lot if suitable (determination of suitability by Departmental Representative) and backfill at McLean Pt if not suitable for Grice Bay parking lot.

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**IN:** 01 32 16.07 CONSTRUCTION PROGRESS 1.6.3.10

**DELETE:** 'Movement of woody debris from the Radar Hill parking lot'

**REPLACE WITH:** 'Movement of woody debris and stockpiled aggregate from the Radar Hill parking lot'

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**IN:** 01 32 16.07 CONSTRUCTION PROGRESS 1.6.3.11

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Solicitation No. - N° de l'invitation  
5P468-21-0110/A

Amd. No. - N° de la modif.  
003

Contracting Authority  
Rebecca Chen

Client Ref. No. - N° de réf. du client  
1568

File Name - Nom du dossier  
Grice Bay & McLean Point Remediation – Pacific Rim National Park Reserve

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**DELETE:** 'Placement of woody debris and site rehabilitation'

**REPLACE WITH:** 'Placement of woody debris and aggregate material and site rehabilitation'

**ALL OTHER TERMS & CONDITIONS REMAIN UNCHANGED**